

31. Poppleton Fire Station (Engine House No. 38)

756-760 Baltimore Street

MIHP No. B-3693

The Poppleton Fire Station (Engine House No. 38), built in 1910, is an exceptional example of Tudor Revival architecture, demonstrating a rather strict academic interpretation of a Tudor gatehouse applied to a fire station. Character-defining features include the brick and limestone facade and central Tudor-arch entrance flanked by crenellated turrets. The Poppleton Fire Station (Engine House No. 38) was listed in the NRHP in 1983 under Criteria A and C. It is listed under Criterion A for its association with the Baltimore Fire Department's period of expansion and demand for excellence and state-of-the-art facilities after the city's devastating downtown fire in 1904. It is also listed under Criterion C as one of the city's best examples of Tudor Revival architecture executed in a historically accurate design. The historic boundary comprises the entirety of the building's parcel.

Red Line Project activity near the Poppleton Fire Station would be extensive, but no work would occur within the property's historic boundary. The Red Line Poppleton Station would be built directly west of the fire station and would require the demolition of two adjacent non-historic buildings that were determined not eligible for the NRHP. The station would be underground and would be constructed using cut-and-cover tunneling techniques that would adjoin the alignment's bored tunnel. Station construction would require substantial disruption on Baltimore Street and Fremont and Fairmount avenues, which surround the block containing the fire station on three sides. Additionally, station entrance construction would occur on the parcel directly west of the Poppleton Fire Station. Entrance features built on this adjacent parcel would include escalators as well as a fan plant/vent structure that would be four to six stories in height; the precise appearance of these structures, including cladding materials, has not been determined but it would have a square footprint measuring approximately 50 feet by 50 feet. Construction activities are anticipated to last for approximately four years; because of this time span, they are not considered temporary impacts.

No physical impacts to the Poppleton Fire Station would occur; no project activity is proposed within the property's NRHP boundary. The arch that connects to the adjacent building would not be impacted during construction; this contributing feature would remain in place. Therefore, no effects to the property's integrity of location, design, materials, or workmanship would occur.

The property's integrity of setting has been diminished over time by new high-rise construction to the west on Fremont Avenue and the presence of six-lane Martin Luther King, Jr. Boulevard to the east. Although the adjacent buildings located directly west of the Poppleton Fire Station are not NRHP eligible, they are low-scale and are of a similar age. Removing these buildings, which are the final vestige of the property's immediate historic setting, and replacing them with station features, most notably the tall fan plant/vent structure, would result in a cumulative adverse effect to the Poppleton Fire Station's setting.

The Poppleton Fire Station retains integrity of feeling and association. Character-defining features that convey the fire station's expression of its aesthetic and period in time, as well as its association with fire fighting in Baltimore, are present and would not be affected by the Red Line

Project. Therefore, the project would have no effect on the Poppleton Fire Station's integrity of feeling and association.

Based on this evaluation, the Red Line Project would have an **adverse effect** to the Poppleton Fire Station (Engine House No. 38).



Figure 196. View northeast toward the proposed tunnel alignment (beneath North Fremont Avenue) and the Poppleton Fire Station (easternmost building); buildings to its west would be demolished for the Poppleton Station fan plant/vent structure, entrance construction, and staging area



Figure 197. View west from the Poppleton Fire Station toward the proposed tunnel alignment beneath North Fremont Avenue



Figure 198. View southwest from the Poppleton Fire Station toward the proposed tunnel alignment beneath North Fremont Avenue

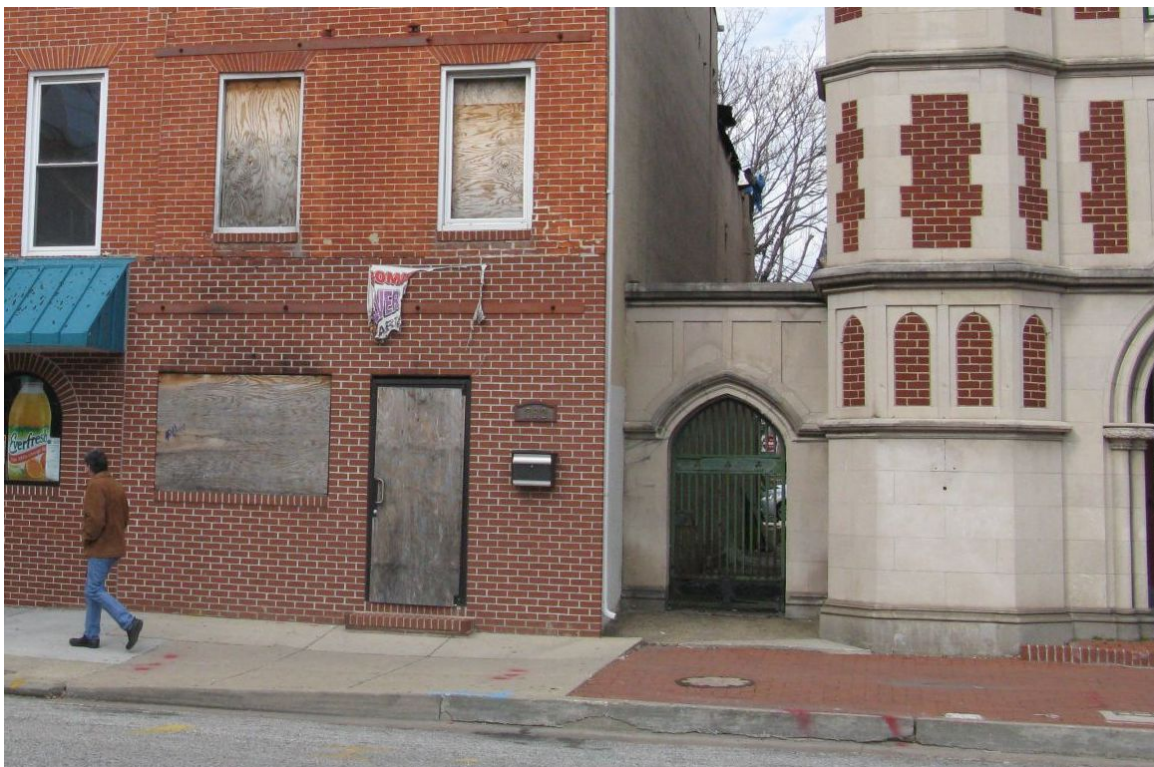


Figure 199. View north toward the Poppleton Fire Station arch connecting to the adjacent building that would be demolished for Poppleton Station entrance construction and staging area

32. St. Paul's Cemetery (Old St. Paul's Cemetery)

Bounded by West Redwood Street, Martin Luther King Jr. Boulevard, West Lombard Street, and the University of Maryland Medical Center campus
MIHP No. B-3686

St. Paul's Cemetery (Old St. Paul's Cemetery) is a 2.4-acre burial ground and one of the two oldest cemeteries in Baltimore. Laid out circa 1799, the cemetery utilizes a grid pattern to organize plots, and burials are marked with conservative stone tablets or monuments. St. Paul's Cemetery was developed during the transitional period between churchyard burial grounds of the eighteenth century and the rural cemetery movement of the nineteenth century. The western edge of the cemetery was destroyed by a 1970s highway project. St. Paul's Cemetery (Old St. Paul's Cemetery) was listed in the NRHP under Criteria A and C and Criteria Consideration D for its association with the transitional period in cemetery development and its conservative funerary art and sculptural markers used during that period.

Red Line Project components would be underground in the vicinity of St. Paul's Cemetery. Underground project work would occur beneath Martin Luther King Jr. Boulevard, passing through the property's western and southeastern NRHP boundaries, and turning east toward West Lombard Street. No proposed stations are proximate to the property.

Although tunneling would occur beneath St. Paul's Cemetery and the NRHP boundary, tunneling activities would occur at a depth significantly below grade and significantly below cemetery burials. No physical impacts to St. Paul's Cemetery or any graves would occur. Therefore, no effects to the property's integrity of location, design, materials, and workmanship would occur.

St. Paul's Cemetery no longer retains integrity of setting due to the construction of six-lane Martin Luther King Jr. Boulevard immediately west of the cemetery and the University of Maryland Medical Center campus, which contains numerous non-historic buildings, to the cemetery's immediate north, south, and east. Furthermore, all project components would be located underground and would not be visible. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property's setting would be impacted. Because no historically significant views would be obscured, no visual impacts to the district were identified. Therefore, project implementation would have no effect to St. Paul Cemetery's integrity of setting.

Furthermore, no project activity would alter the property's feeling as a late-eighteenth-to-early-nineteenth century cemetery, or its association with the transitional period between churchyard burial grounds and the rural cemetery movement. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to St. Paul's Cemetery (Old St. Paul's Cemetery).

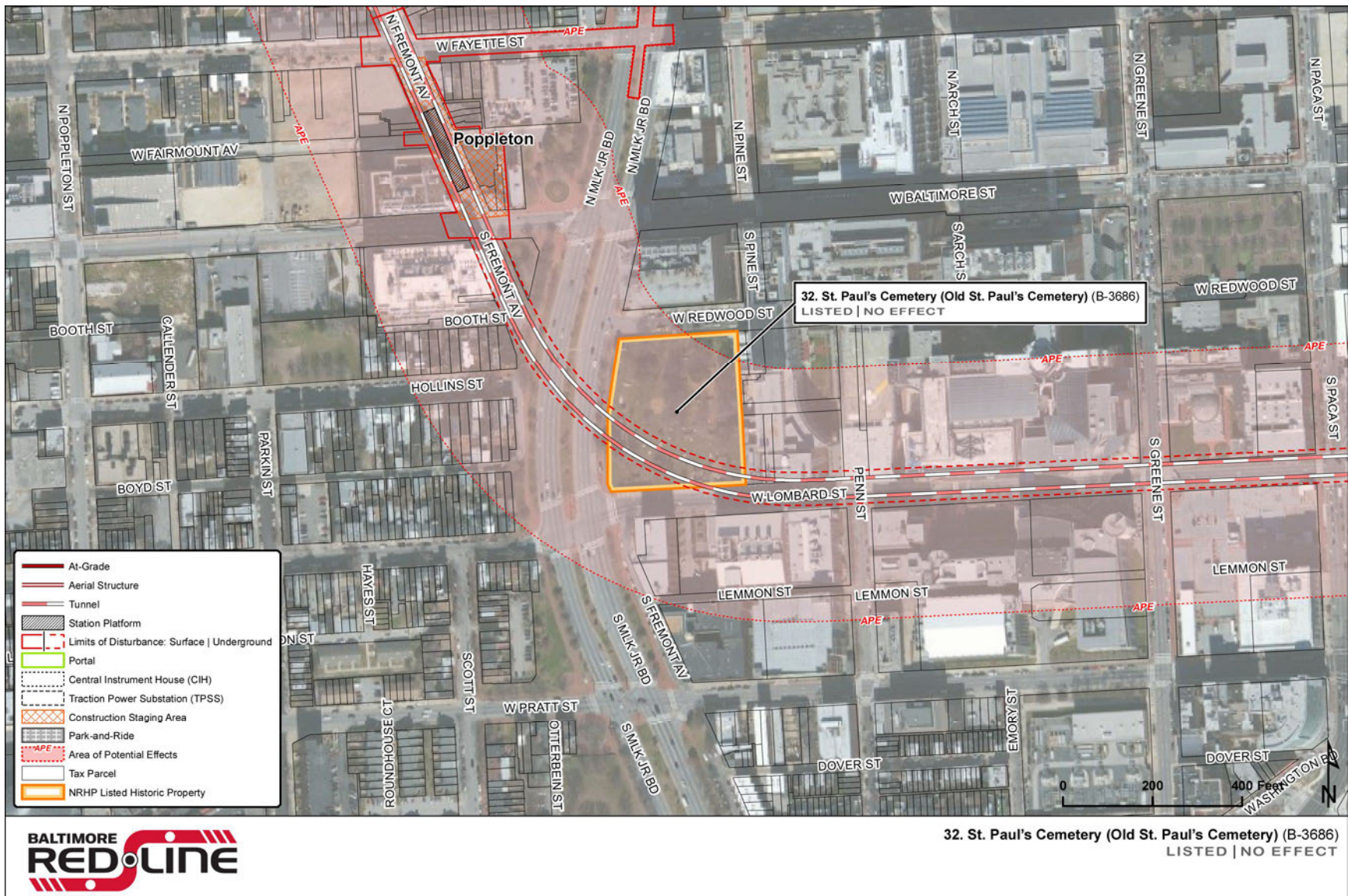


Figure 200. Proposed project in vicinity of St. Paul's Cemetery (Old St. Paul's Cemetery)



Figure 201. View northwest from St. Paul's Cemetery toward the proposed tunnel alignment beneath Martin Luther King Jr. Boulevard transitioning from beneath North Fremont Avenue



Figure 202. View northeast to St. Paul's Cemetery at proposed tunnel alignment transition from beneath Martin Luther King Jr. Boulevard to beneath cemetery



Figure 203. View northwest to St. Paul's Cemetery from West Lombard Street; proposed tunnel beneath cemetery



Figure 204. View east to St. Paul's Cemetery with the proposed tunnel alignment transitioning from beneath cemetery to beneath West Lombard Street



Figure 205. View northeast toward the proposed tunnel alignment beneath St. Paul's Cemetery transitioning to beneath West Lombard Street

33. Barre Circle Historic District

Roughly bounded by Martin Luther King Jr. Boulevard, Boyd Street, Scott Street, and Ramsey Street

MIHP No. B-3701

The Barre Circle Historic District is a collection of mid-nineteenth century row houses with minimal ornamentation. These modest row houses were rapidly constructed to accommodate an increased need for workers' housing during the period. Baltimore experienced rapid industrial expansion in the nineteenth century which caused an influx of workers to the city. The Barre Circle Historic District was listed in the NRHP under Criterion A for its association with industrial development of Baltimore and under Criterion C for its unusually modest row house architecture.

Red Line Project components would be underground in the vicinity of the Barre Circle Historic District. Underground project work would occur beneath Martin Luther King Jr. Boulevard approximately 170 feet from the district's northeast NRHP boundary.

No physical impacts to the Barre Circle Historic District would occur; no project activity is proposed within the district's NRHP boundary. Therefore, no effects to the district's integrity of location, design, materials, and workmanship would occur.

Project implementation would not affect the Barre Circle Historic District's setting as all project components would be located underground and would not be visible. No historically significant views to or from the district would be obscured by project implementation, and no character-defining features of the district's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the district were identified. Therefore, project implementation would have no effect to the Barre Circle Historic District's integrity of setting.

Furthermore, no project activity would alter the district's feeling as a nineteenth-century neighborhood with modest row houses, or its association with that building type or Baltimore's nineteenth-century industrial development. Therefore, project implementation would have no effect to the district's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the Barre Circle Historic District.

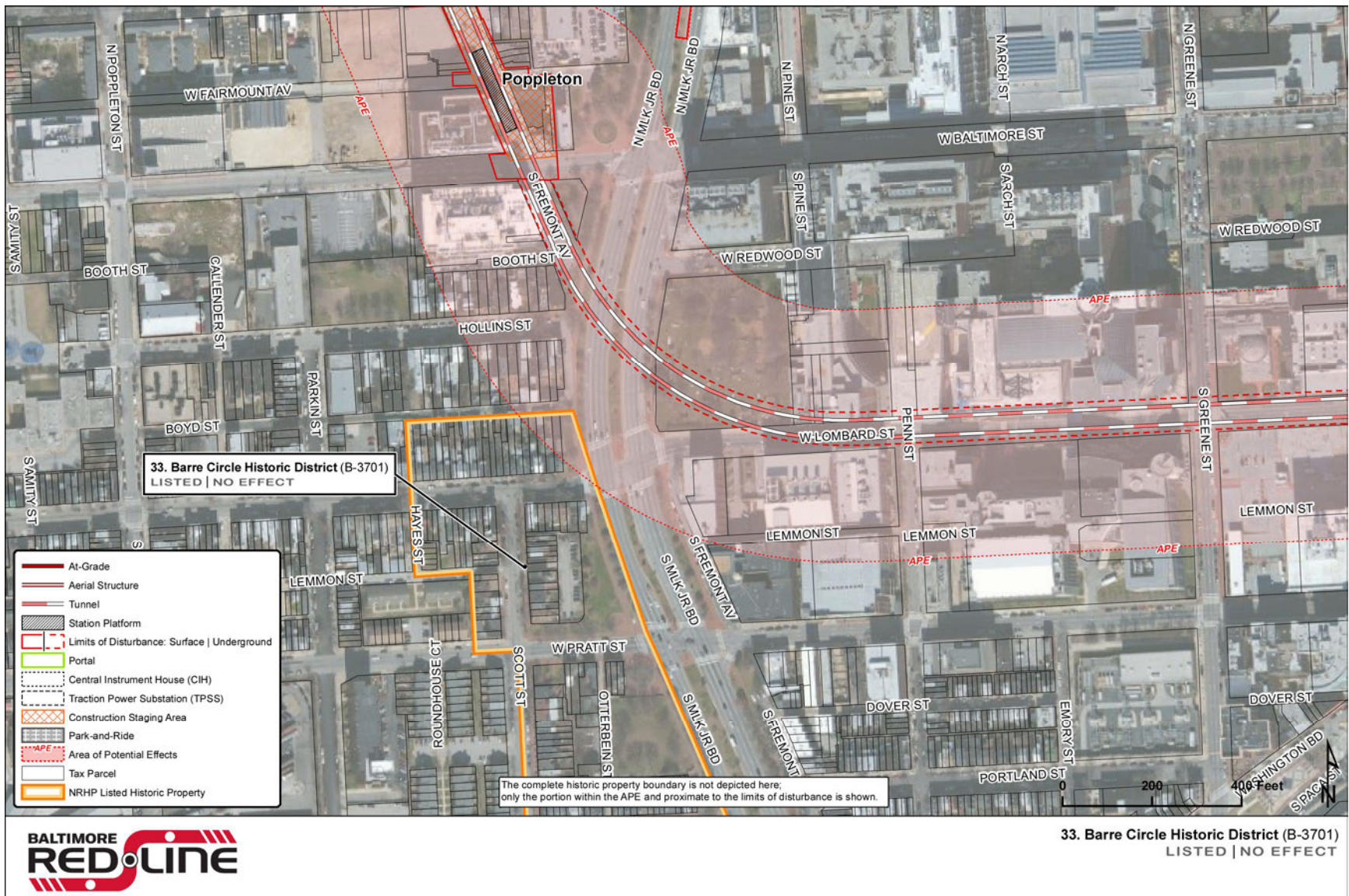


Figure 206. Proposed project in vicinity of the Barre Circle Historic District



Figure 207. View southwest toward the proposed tunnel alignment beneath Martin Luther King Jr. Boulevard and to the Barre Circle Historic District from West Lombard Street



Figure 208. View northeast from Barre Circle Historic District (at West Lombard Street) toward proposed alignment transitioning from beneath Martin Luther King Jr. Boulevard to beneath St. Paul's Cemetery



Figure 209. View northwest toward the Barre Circle Historic District (at West Lombard Street) and the proposed tunnel alignment beneath St. Paul's Cemetery



Figure 210. View northeast toward the proposed tunnel alignment beneath St. Paul's Cemetery from the Barre Circle Historic District at West Lombard Street

34. University of Maryland-Fremont Building

737 West Lombard Street

MIHP No. B-3594

The University of Maryland-Fremont Building is a large, six-story, brick commercial loft building; all window openings have segmental arches. Its five-story primary facade has a symmetrical tripartite division, each containing three bays of windows in the upper stories; three segmental-arched storefronts are located on the first floor with stone keystones. The building's top floor also incorporates a series of projecting brick belt courses and is interspersed with terra cotta ornamental panels. The cornice is tin-faced, projecting, boxed cornice with scalloped detailing along its ledge. Side elevations are less articulated but contain segmental-arched windows and recesses. The building was constructed in 1906 to serve as a new factory and headquarters for the Carriage and Toy Company, a Baltimore-based manufacturer of dolls and baby carriages. The building was designed by J. E. Laferty and constructed by Henry S. Ripple. The company was a national supplier of toys and had grown to be the country's third largest toy manufacturer by 1922; however, due to debt, the company sold this building at auction in 1927. After serving as a furniture warehouse, the building was acquired by the University of Maryland in 1975 and has served an institutional purpose since. The building has been altered, including window and entrance replacements, but these changes have been compatible. The University of Maryland-Fremont Building was determined eligible for the National Register under Criterion C in the area of architecture as a good example of loft-style brick warehouses constructed in Baltimore during the late-nineteenth- and early twentieth centuries.

All Red Line Project components would be underground in the University of Maryland-Fremont Building's vicinity; no project features would be aboveground or visible. Project work would occur approximately 70 feet north of the University of Maryland-Fremont Building's north side elevation and north historic property boundary, and beneath West Lombard Street.

No physical impacts to the University of Maryland-Fremont Building would occur; no project activity is proposed within the property's NRHP boundary. Therefore, no effects to the property's integrity of location, design, materials, and workmanship would occur.

The University of Maryland School-Fremont Building does not retain integrity of setting due to the construction of six-lane Martin Luther King Jr. Boulevard immediately west of the property and other non-historic buildings within the University of Maryland Medical Center campus to the property's east. The NRHP-listed St. Paul's Cemetery and the Gandy Belting Company Building are located to the property's north and south, respectively. All proposed project components would be located underground and would not alter the property's visual setting. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the property were identified. Therefore, project implementation would have no effect to the University of Maryland-Fremont Building's integrity of setting.

Furthermore, no project activity would alter the property's feeling as an early-twentieth-century, loft-style brick warehouse, or its association with that building type in Baltimore. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the University of Maryland-Fremont Building.

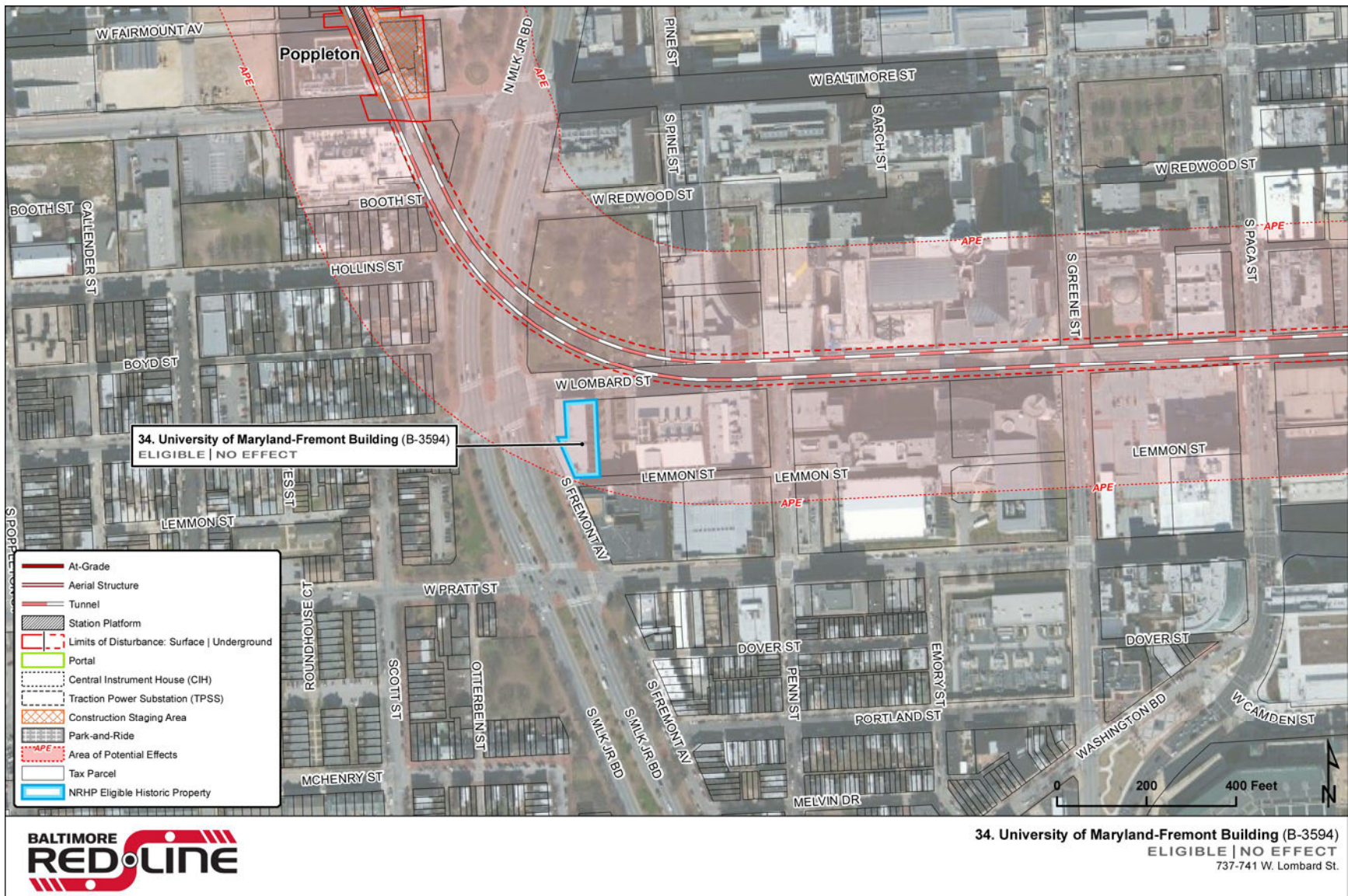


Figure 211. Proposed project in vicinity of University of Maryland-Fremont Building



Figure 212. View east toward the Fremont Building (southernmost building) as the proposed tunnel alignment transitions from beneath St. Paul's Cemetery to beneath West Lombard Street



Figure 213. View north from the Fremont Building toward the proposed tunnel alignment transitioning from beneath Martin Luther King Jr. Boulevard to beneath St. Paul's Cemetery



Figure 214. View northwest from the Fremont Building toward the proposed tunnel alignment beneath St. Paul's Cemetery

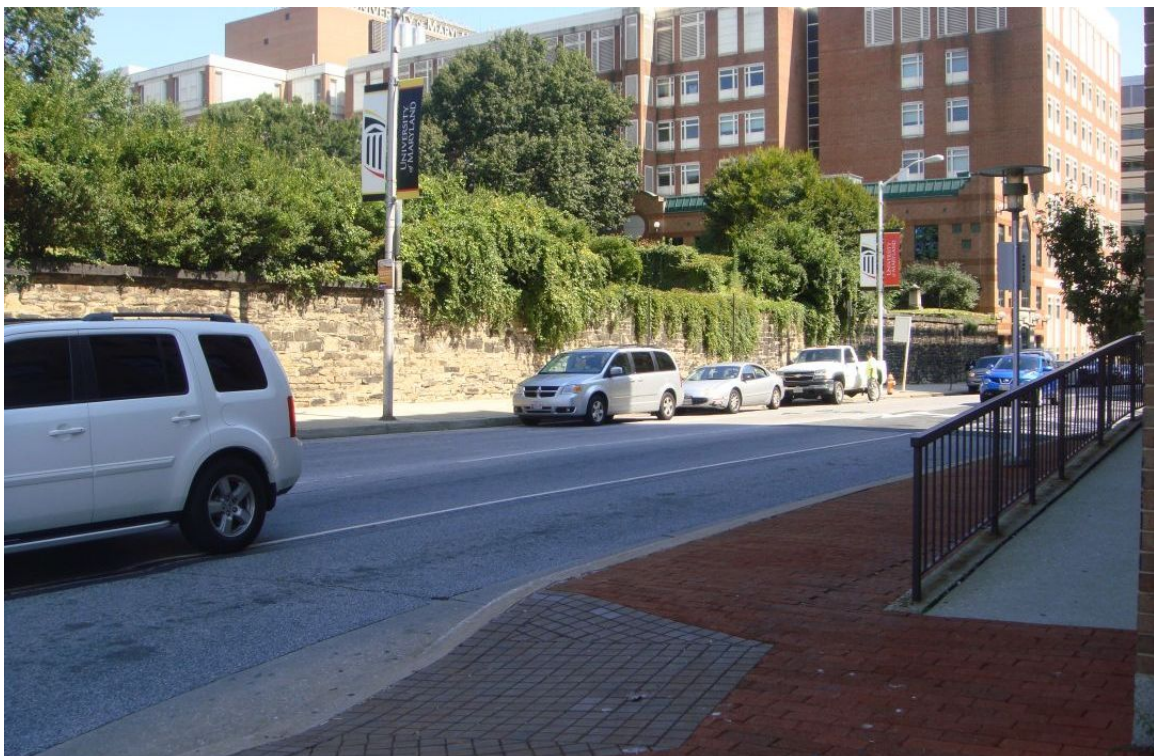


Figure 215. View northeast from the Fremont Building toward the proposed tunnel alignment transitioning from beneath St. Paul's Cemetery to beneath West Lombard Street

35. Gandy Belting Company Building

726-734 West Pratt Street

MIHP No. 4092

The Gandy Belting Company Building is a five-story, brick and stone building constructed in five sections between 1888 and 1911. The building exhibits minimal ornamentation, primarily in the form of dentils near the cornice, segmented and rounded arch entries along West Pratt Street, and rusticated window sills and lintels. Gandy Belting Company produced machinery belting that helped transition Baltimore's textile industry from nineteenth-century manual techniques to twentieth-century, machine-driven manufacture. Standing as an example of the early elevator factories once prevalent around downtown, the Gandy Belting Company Building was an integral part of Baltimore's former manufacturing and warehouse district along the old Baltimore and Ohio Railroad tracks. The Gandy Belting Company Building was listed in the NRHP under Criterion A for its association with Baltimore's textile industry and its transition to twentieth century technologies and under Criterion C as a rare, early elevator factory building in Baltimore.

Red Line Project components would be underground in the vicinity of the Gandy Belting Company Building. Underground project work would occur beneath West Lombard Street, approximately 215 feet north of the property's northern NRHP boundary. No proposed stations are proximate to the property.

No physical impacts to the Gandy Belting Company Building would occur; no project activity is proposed within the property's NRHP boundary. Therefore, no effects to the property's integrity of location, design, materials, and workmanship would occur.

Project implementation would not affect the Gandy Belting Company Building's setting as all project components would be located underground and would not be visible. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the property were identified. Therefore, project implementation would have no effect to the Gandy Belting Company Building's integrity of setting.

Furthermore, no project activity would alter the property's feeling as a late-nineteenth to early twentieth-century factory building, or its association with that building type or Baltimore's industrial heritage. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the Gandy Belting Company Building.

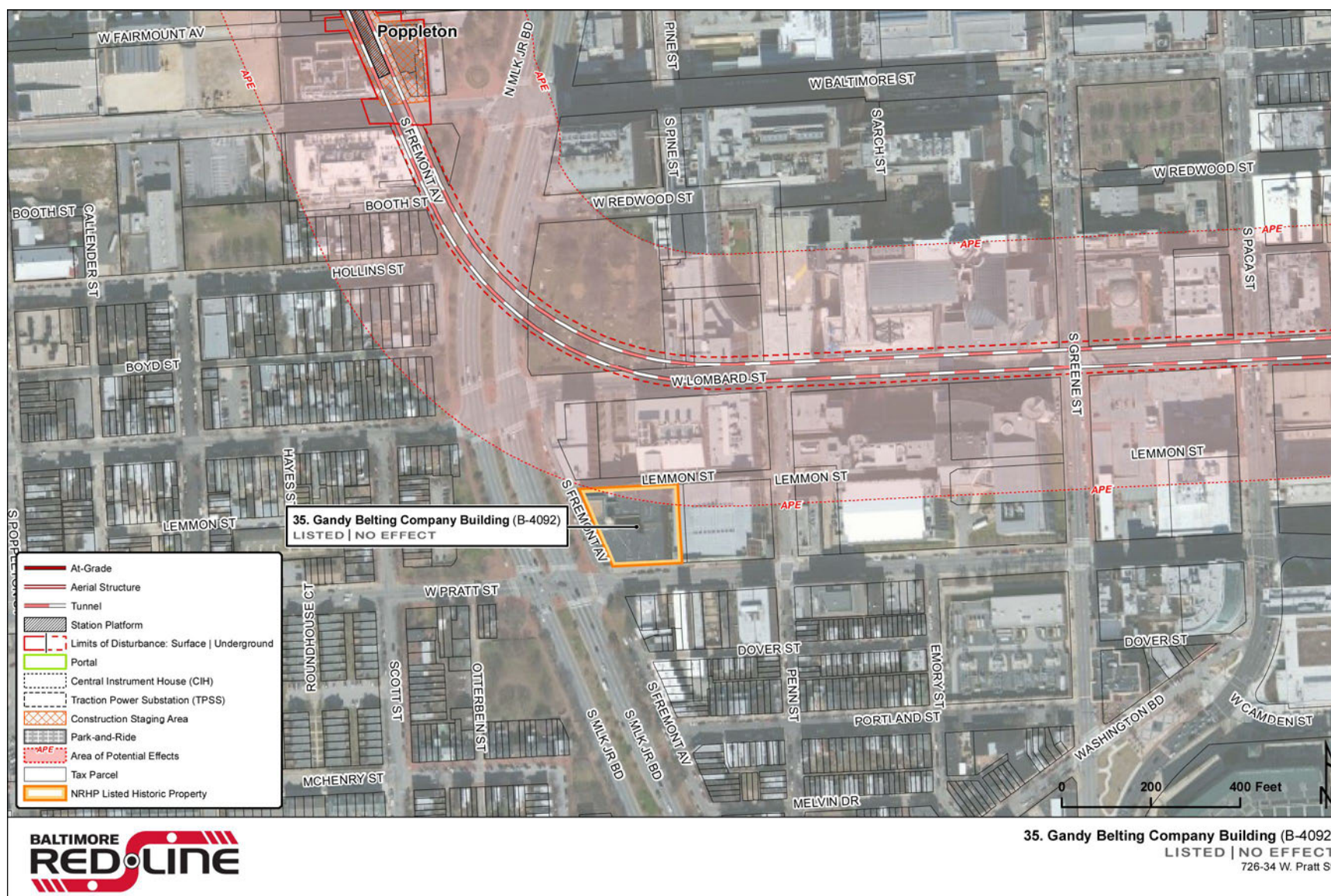




Figure 217. View south from the proposed tunnel alignment (beneath West Lombard Street) toward the rear of the Gandy Belting Company Building (in the center background)

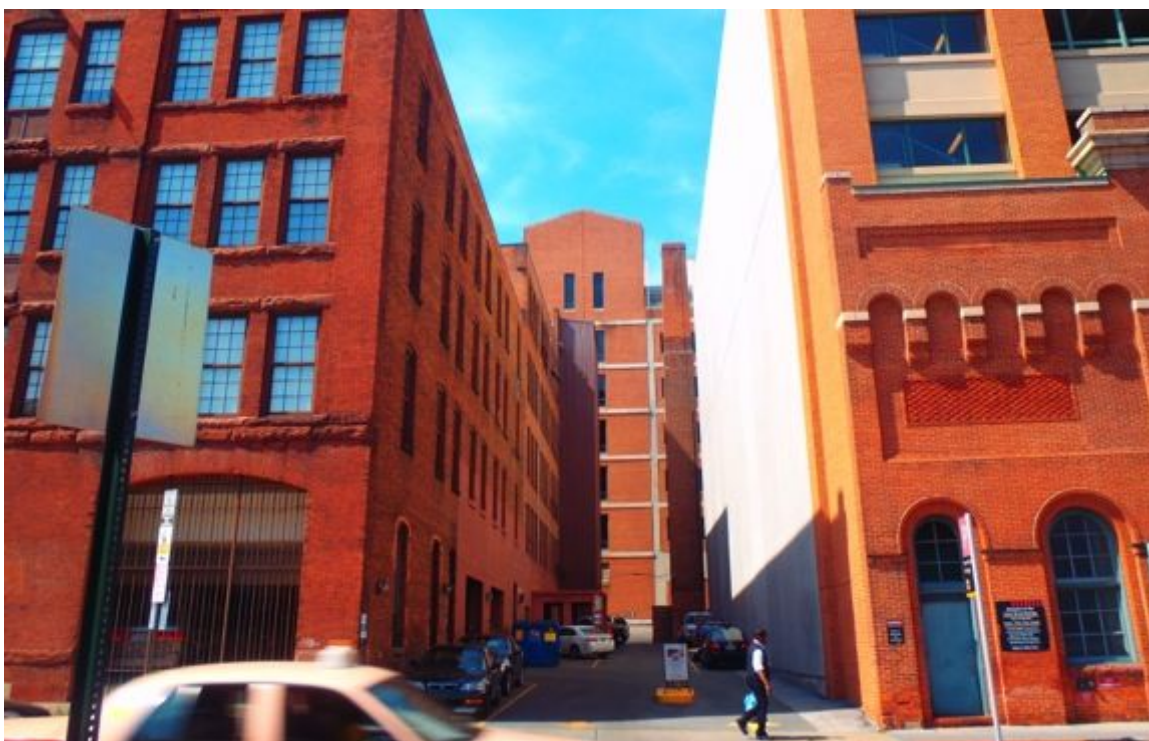


Figure 218. View north toward the proposed tunnel alignment (beneath West Lombard Street), with the Gandy Belting Company Building in west foreground

36. Penn Street Sub-Station (Baltimore Gas and Electric Substation)

700-726 West Pratt Street

MIHP No. B-1053

The Penn Street Sub-Station (Baltimore Gas and Electric Substation) consisted of two buildings connected along West Pratt Street but now exists as facades only. Both facades are brick and emphasize arches in their design, indicating the growing influence of architects on industrial design during the late nineteenth century. The substation played an important role in Baltimore's early use of electricity, being the primary source of power for the city's western districts and the nearby industrial and manufacturing district. The Penn Street Sub-Station was determined eligible for listing in the NRHP under Criterion A for its association with the development of electrical power in Baltimore in the late nineteenth and early twentieth centuries.

Red Line Project components would be underground in the vicinity of the Penn Street Sub-Station. Underground project work would occur beneath West Lombard Street approximately 210 feet from the property's north NRHP boundary. No proposed stations are proximate to the property.

Penn Street Sub-Station no longer retains integrity of design, materials, or workmanship. The building exists only as a facade fronting West Pratt Street, with the remaining building replaced by a non-historic parking garage. Furthermore, no physical impacts to the property would occur; no project activity is proposed within the property's NRHP boundary. Therefore, no effects to the property's integrity of location, design, materials, and workmanship would occur.

Project implementation would not affect the Penn Street Sub-Station's setting as all project components would be located underground and would not be visible. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the property were identified. Therefore, project implementation would have no effect to the Penn Street Sub-Station's integrity of setting.

Because only the Penn Street Sub-Station's facade remains and the remaining power substation has been razed and replaced by a parking garage, the substation no longer retains integrity of feeling or association. The substation no longer retains its feeling as a late-nineteenth to early twentieth-century power substation or its association with the development of electrical power in Baltimore during that period. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the Penn Street Sub-Station (Baltimore Gas and Electric Substation).



Figure 220. View south from the proposed tunnel alignment (beneath West Lombard Street) toward the Penn Street Sub-Station (beyond non-historic building in photograph)

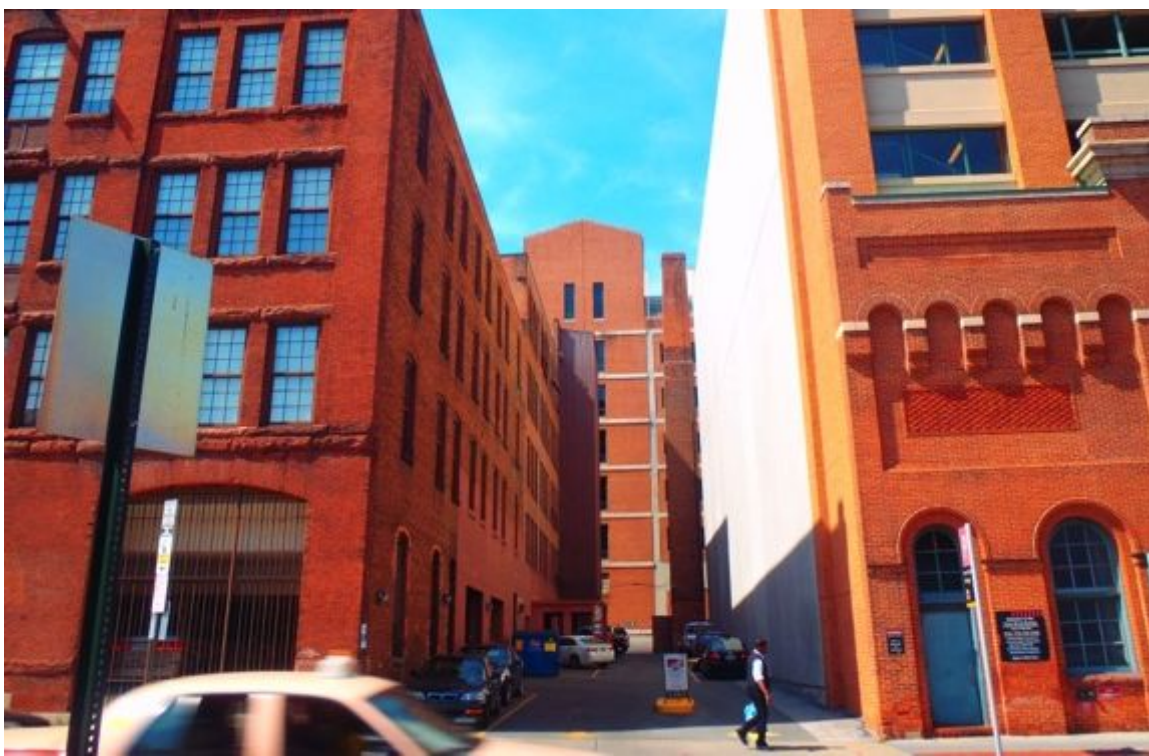


Figure 221. View north toward the proposed tunnel alignment (beneath West Lombard Street), with the Penn Street Sub-Station facade in east foreground

37. University of Maryland Medical School-Hospital District

South Green Street, West Redwood Street, and West Lombard Street
MIHP No. B-5128

The University of Maryland Medical School-Hospital District is a collection of seven university buildings varying in styles and constructed between 1812 and 1934; they represent early Baltimore professional schools and the historic core of the University of Maryland at Baltimore campus. A medical school was opened in Davidge Hall in 1812 and was later joined by the law school, dental school, pharmacy school, and other graduate programs. In 1920, the professional schools merged with Maryland State College in College Park to form the University of Maryland system. Architectural styles represented in the district include Neoclassical, Gothic Revival, Beaux Arts, and Colonial Revival. District buildings include Davidge Hall, also a National Historic Landmark and listed individually in the NRHP; University of Maryland Law School/University College/East Hall; Gray Laboratory; Dental-Pharmaceutical Building/Medical-Technical Building/University of Maryland Department of Preventative Medicine/National Museum of Dentistry; University of Maryland Law School/University of Maryland School of Social Work; University of Maryland Bressler Memorial Library; and University Hospital. The district was determined eligible for listing in the NRHP under Criteria A for its association with the historic development of the University of Maryland and its Baltimore campus.

Red Line Project components would be underground in the vicinity of the University of Maryland Medical School-Hospital District. Underground project work would occur beneath West Lombard Street, bisecting the district's NRHP boundaries. No proposed stations are proximate to the district.

Although tunneling would occur beneath the University of Maryland Medical School-Hospital District and within the NRHP boundary, no physical impacts to the district would occur. Therefore, no effects to the district's integrity of location, design, materials, and workmanship would occur.

Project implementation would not affect the University of Maryland Medical School-Hospital District's setting as all project components would be located underground and would not be visible. No historically significant views to or from the district would be obscured by project implementation, and no character-defining features of the district's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the district were identified. Therefore, project implementation would have no effect to the University of Maryland Medical School-Hospital District's integrity of setting.

Furthermore, no project activity would alter the district's feeling as a university campus or its association with the development of the University of Maryland in Baltimore. Therefore, project implementation would have no effect to the district's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the University of Maryland Medical School-Hospital District.

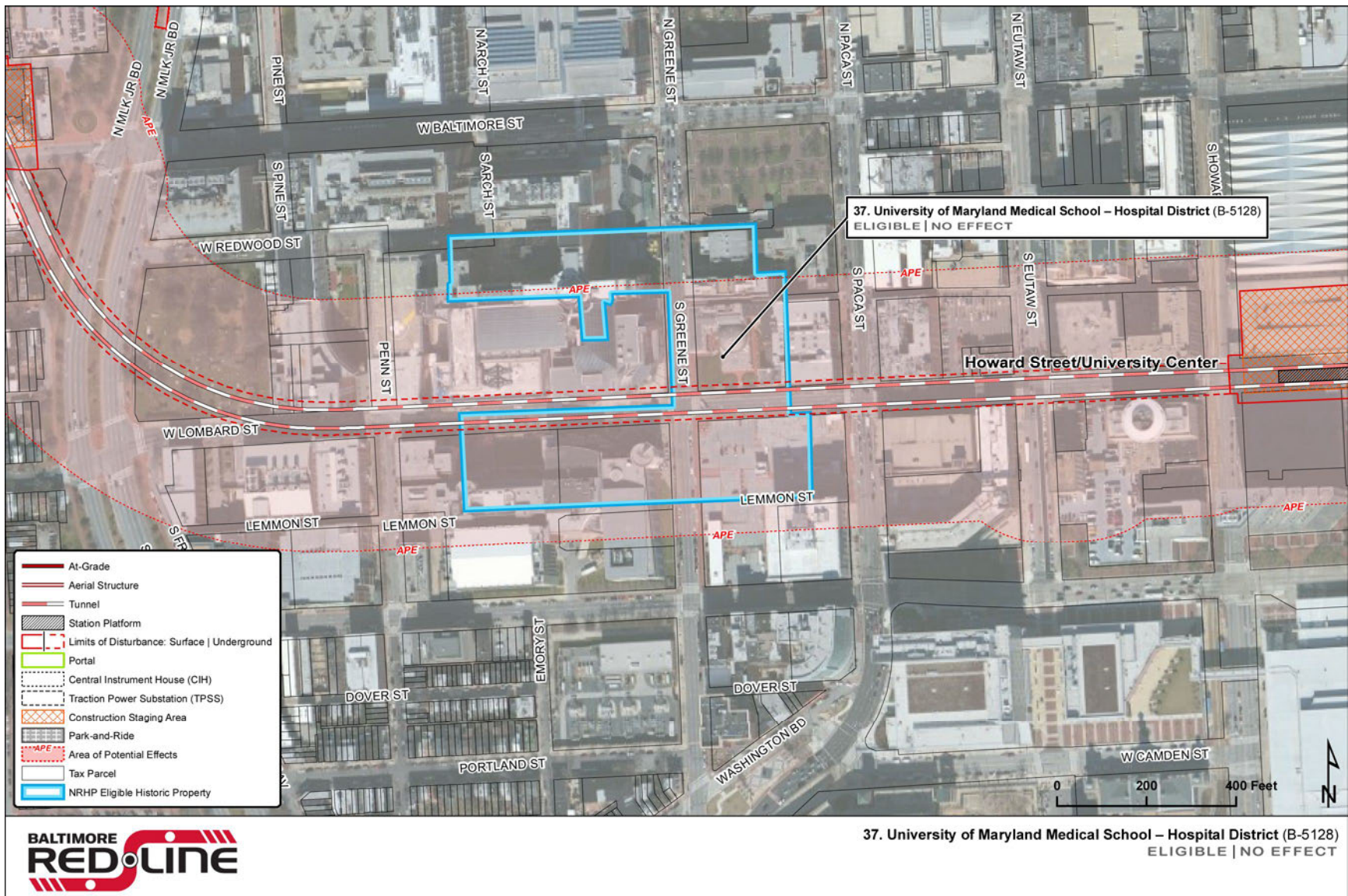


Figure 222. Proposed project in vicinity of the University of Maryland Medical School-Hospital District



Figure 223. View northeast from the proposed tunnel alignment (beneath West Lombard Street) toward the University of Maryland Medical School-Hospital District along South Greene Street



Figure 224. View south toward the proposed tunnel alignment (beneath West Lombard Street) from within the University of Maryland Medical School-Hospital District along South Greene Street



Figure 225. View west from within the University of Maryland Medical School-Hospital District, along the proposed tunnel alignment (beneath West Lombard Street) at the South Greene Street intersection



Figure 226. View northwest toward the proposed tunnel alignment (beneath West Lombard Street) from within the University of Maryland Medical School-Hospital District (between South Greene and South Paca streets)



Figure 227. View northwest along the proposed tunnel alignment (beneath West Lombard Street) from within the University of Maryland Medical School-Hospital District (near North Paca Street)



Figure 228. View northeast toward the proposed tunnel alignment (beneath West Lombard Street) from within the University of Maryland Medical School-Hospital District (between South Greene and South Paca Streets)

38. UMD-Bressler Memorial Laboratory Building

29 South Greene Street

MIHP No. B-3589

The UMD-Bressler Memorial Laboratory Building is a six-story, oblong-shaped, tiered Art Deco-style medical school building clad in brick with limestone and concrete trim. The building's flat roof terminates in a shallow-pitched, gable-pedimented parapet decorated by scallop shell ornamental finials at the center peak and each end of the pediment. It was constructed in 1939 to house the university's preclinical departments and was named in honor of Dr. Frank C. Bressler who worked at University Hospital and donated \$1 million to the medical school upon his death in 1935. The building was a component of the 1930s expansion of university and hospital facilities in the surrounding blocks. The UMD-Bressler Memorial Laboratory Building was determined eligible for listing in the NRHP under Criterion A for its association with the 1930s expansion of the University of Maryland at Baltimore campus' educational and hospital facilities, and under Criterion C as a good example of an Art Deco-style medical school building. It is also located within the NRHP-eligible University of Maryland Medical School – Hospital District.

All Red Line Project components would be underground in the UMD-Bressler Memorial Laboratory Building's vicinity; no project features would be aboveground or visible. Project work would occur approximately 205 feet south of the UMD-Bressler Memorial Laboratory Building's rear, south elevation and south historic property boundary, and beneath West Lombard Street.

No physical impacts to the UMD-Bressler Memorial Laboratory Building would occur; no project activity is proposed within the property's NRHP boundary. Therefore, no effects to the property's integrity of location, design, materials, and workmanship would occur.

The UMD-Bressler Memorial Laboratory Building does not retain integrity of setting due to the construction of nearby non-historic buildings, although older buildings remain in its vicinity. All proposed project components would be located underground and would not alter the property's visual setting. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the property were identified. Therefore, project implementation would have no effect to the UMD-Bressler Memorial Laboratory Building's integrity of setting.

Furthermore, no project activity would alter the property's feeling as an Art Deco-style medical school building, or its association with that style and the 1930s expansion of the University of Maryland at Baltimore's campus. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the UMD-Bressler Memorial Laboratory Building.

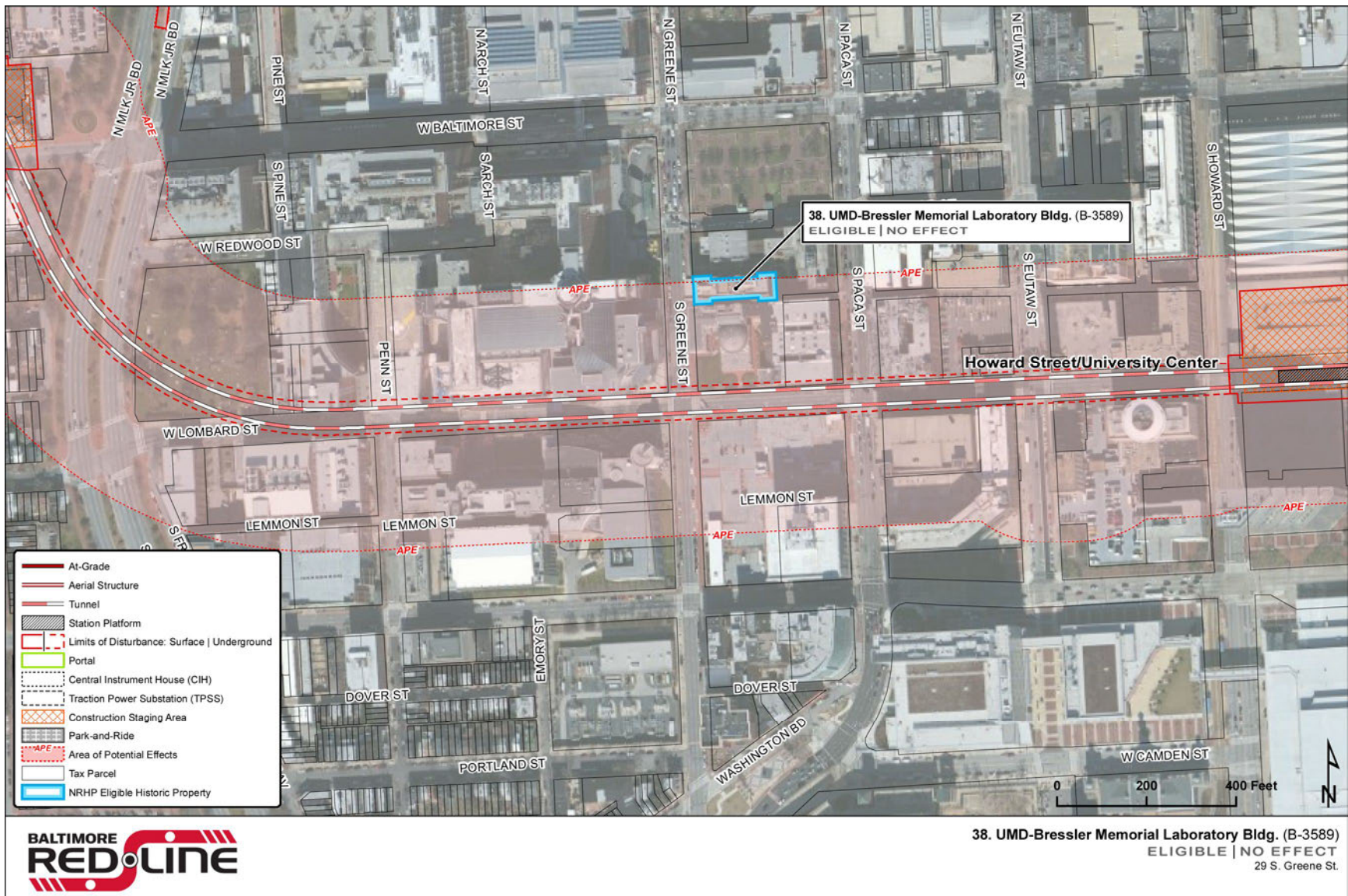


Figure 229. Proposed project in vicinity of UMD-Bressler Memorial Laboratory Building



Figure 230. View northeast along South Greene Street from the proposed tunnel alignment (beneath West Lombard Street) toward the UMD-Bressler Memorial Laboratory Building (see red arrow)



Figure 231. View south toward the proposed tunnel alignment (beneath West Lombard Street), with the UMD-Bressler Memorial Laboratory Building in the east foreground

39. Dental and Pharmaceutical Building (Medical-Technical Building, UMD Dept. of Preventative Medicine)

31 South Greene Street
MIHP No. B-2327

The Dental and Pharmaceutical Building (Medical-Technical Building, UMD Dept. of Preventative Medicine) is an L-shaped, brick and limestone-clad, Renaissance Revival-style medical school building with three full stories on an English basement level. The symmetrical, nine-bay facade features a rusticated basement level, a central entrance bay with a classical door surround, an accentuated cornice, and a roof-line balustrade. Other architectural details include quoins, arched windows, and dentil work. The building was constructed in 1904 as part of the first major building campaign on the University of Maryland at Baltimore's campus and was originally used as the university's Dental and Pharmaceutical Building; it became L-shaped due to a later addition. The Dental and Pharmaceutical Building was determined eligible for listing in the NRHP under Criterion C as an excellent example of the Renaissance Revival style rendered in skillful detail. It is also located within the NRHP-eligible University of Maryland Medical School – Hospital District.

All Red Line Project components would be underground in the Dental and Pharmaceutical Building's vicinity; no project features would be aboveground or visible. Project work would occur approximately 75 feet south of the Dental and Pharmaceutical Building's south elevation and south historic property boundary, and beneath West Lombard Street.

No physical impacts to the Dental and Pharmaceutical Building would occur; no project activity is proposed within the property's NRHP boundary. Therefore, no effects to the property's integrity of location, design, materials, and workmanship would occur.

The Dental and Pharmaceutical Building does not retain integrity of setting due to the construction of non-historic buildings in its vicinity. All proposed project components would be located underground and would not further change the property's altered visual setting. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the property were identified. Therefore, project implementation would have no effect to the Dental and Pharmaceutical Building's integrity of setting.

Furthermore, no project activity would alter the property's feeling as a Renaissance Revival-style medical school building or its association with that style and function. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the Dental and Pharmaceutical Building (Medical-Technical Building, UMD Dept. of Preventative Medicine).

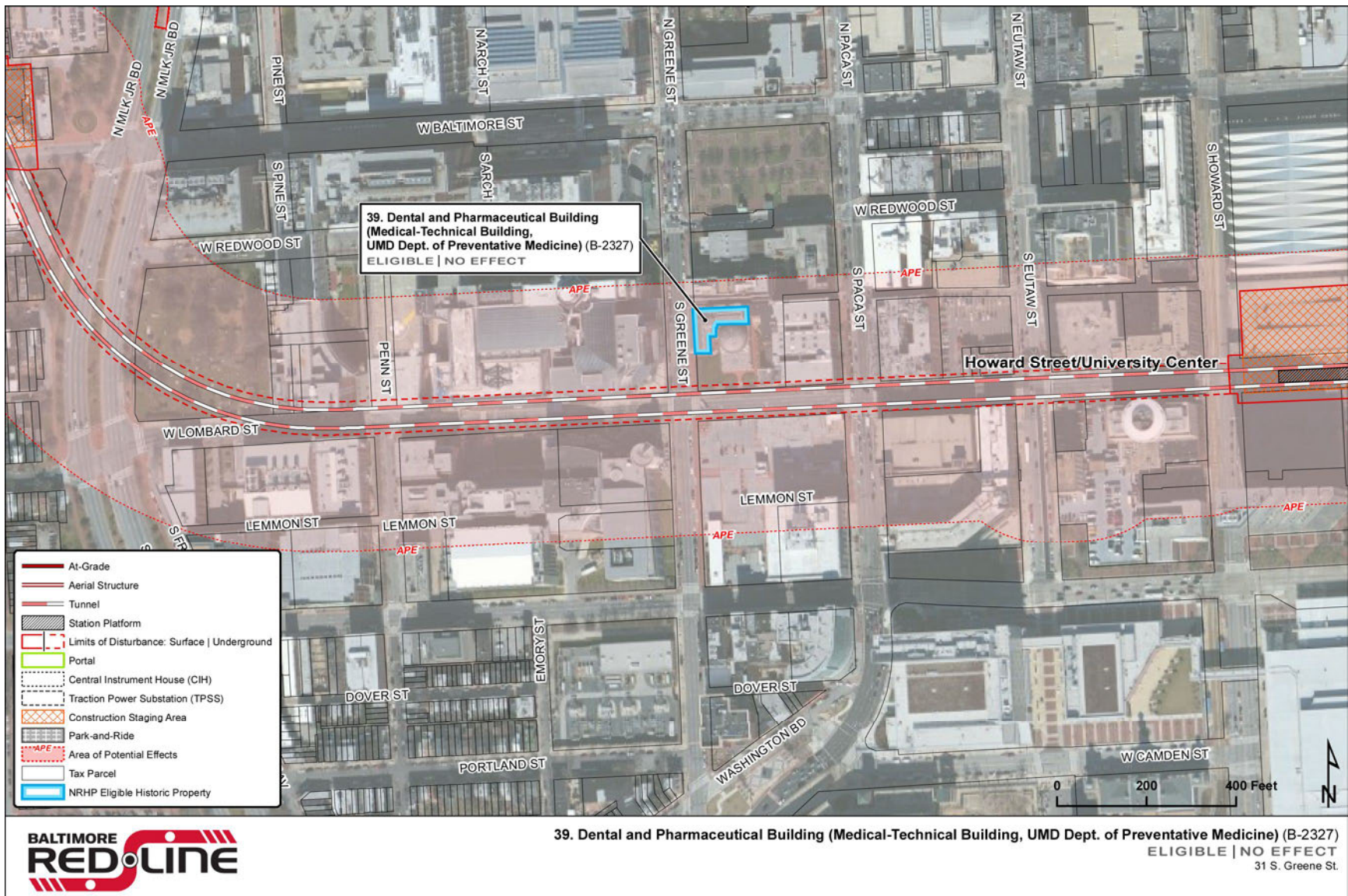


Figure 232. Proposed project in vicinity of Dental and Pharmaceutical Building (Medical-Technical Building, UMD Dept. of Preventative Medicine)



Figure 233. View northeast toward the proposed tunnel alignment (beneath West Lombard Street) and to the Dental and Pharmaceutical Building (see red arrow)



Figure 234. View north toward the proposed tunnel alignment (beneath West Lombard Street) and the Dental and Pharmaceutical Building (in photo center)



Figure 235. View south toward the proposed tunnel alignment (beneath West Lombard Street); Dental and Pharmaceutical Building to east (see red arrow)

40. Davidge Hall, University of Maryland (College of Medicine of Maryland)

522 West Lombard Street

MIHP No. B-41

Davidge Hall, University of Maryland (College of Medicine of Maryland) is a one-story, brick-clad, wood-domed, Greek Revival-style building designed by Robert Cary Long, Sr. after the Pantheon in Rome, Italy. The building facade comprises a wooden portico with a pediment and eight Doric columns. The low, wooden dome rising above a high drum conceals the round shape of the main interior room, which contains two circular amphitheaters that were originally known as “Anatomical Hall” and “Chemical Hall.” Constructed in 1811-1813, it is the oldest building in the United States used continuously for medical education and is named for Dr. John Beale Davidge who was the first dean of the College of Medicine of Maryland. Davidge Hall was listed in the NRHP under Criterion A as the oldest building in the United States which has been used continuously for medical education, and under Criterion C as an excellent example of a Greek Revival-style medical school building inspired by the Pantheon and designed by leading Baltimore-born architect Robert Cary Long, Sr. The building is a National Historic Landmark and located within the NRHP-eligible University of Maryland Medical School – Hospital District.

All Red Line Project components would be underground in Davidge Hall’s vicinity; no project features would be aboveground or visible. Project work would occur approximately 25 feet south of Davidge Hall’s south-facing facade and south historic property boundary, and beneath West Lombard Street.

No physical impacts to Davidge Hall would occur; no project activity is proposed within the property’s NRHP boundary. Therefore, no effects to the property’s integrity of location, design, materials, and workmanship would occur.

Davidge Hall’s integrity of setting is diminished due to the construction of non-historic buildings and a surface parking lot, although older buildings remain intact in its vicinity, including other University of Maryland at Baltimore buildings. All proposed project components would be located underground and would not alter the property’s visual setting. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property’s setting would be impacted. Because no historically significant views would be obscured, no visual effects to the property were identified. Therefore, project implementation would have no effect to Davidge Hall’s integrity of setting.

Furthermore, no project activity would alter the property’s feeling as an early nineteenth-century Greek Revival-style medical education building, or its association with that style or as the oldest building in the United States to be continuously used for medical education. Therefore, project implementation would have no effect to the property’s integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to Davidge Hall, University of Maryland (College of Medicine of Maryland).

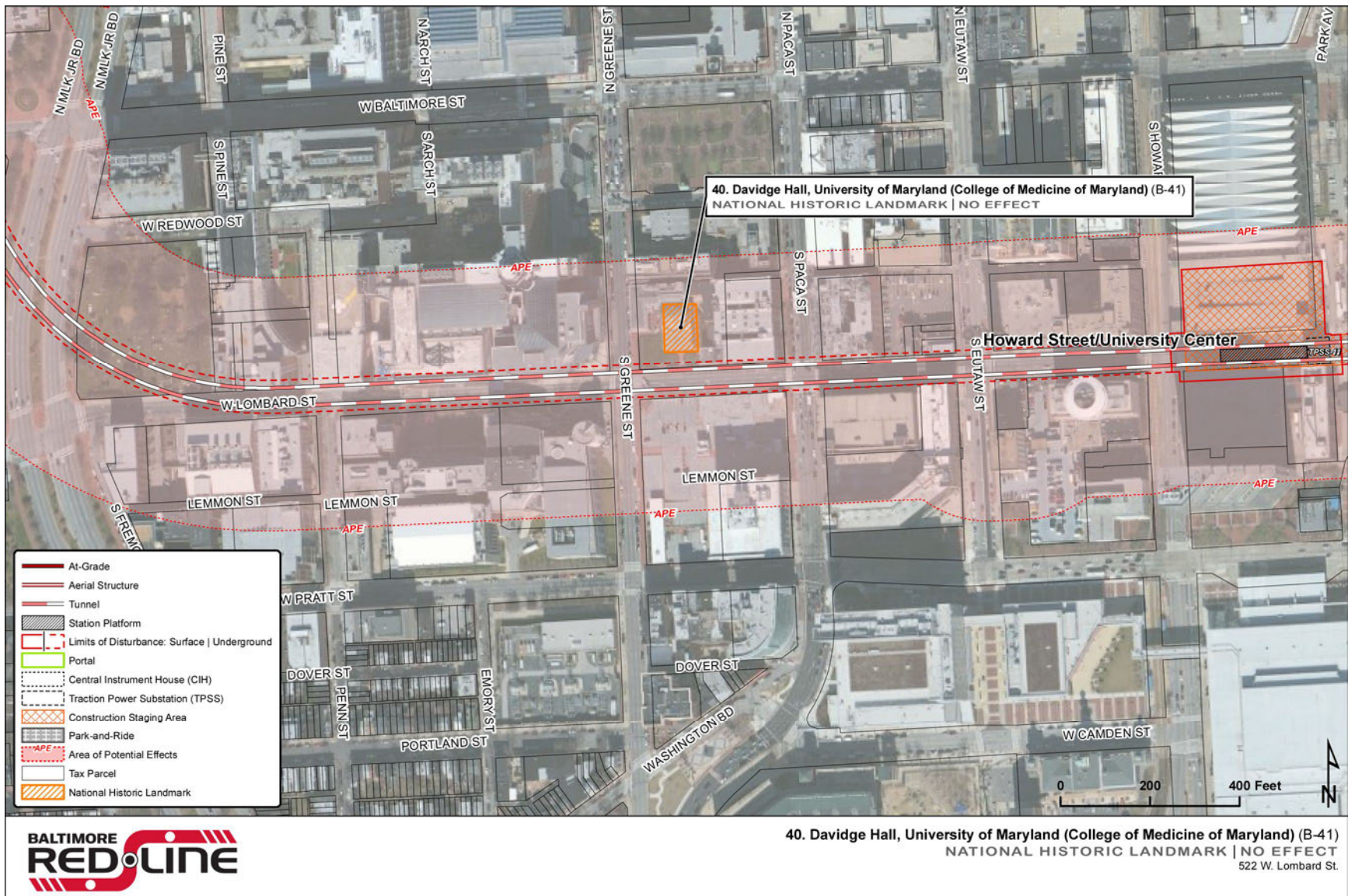


Figure 236. Proposed project in vicinity of Davidge Hall, University of Maryland (College of Medicine of Maryland)



Figure 237. View northeast toward the proposed tunnel alignment (beneath West Lombard Street) and to Davidge Hall, University of Maryland on east (see red arrow)

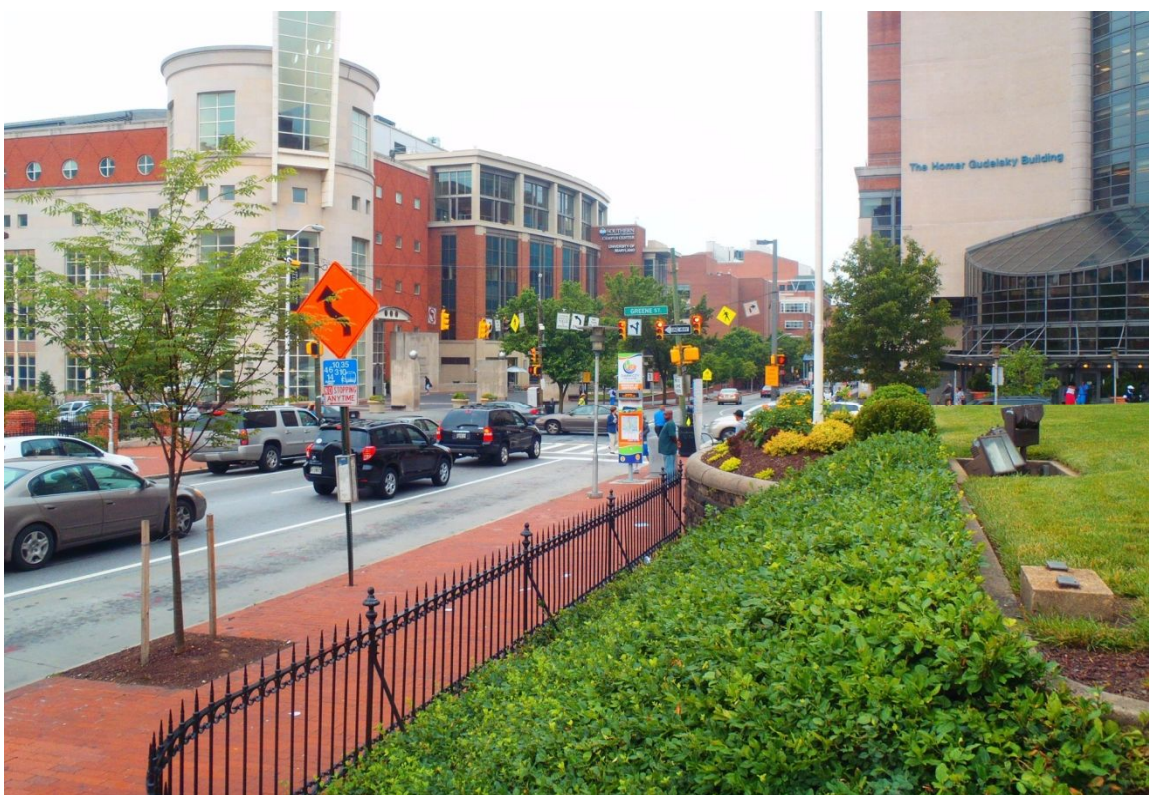


Figure 238. View southwest toward the proposed tunnel alignment (beneath West Lombard Street) from Davidge Hall, University of Maryland



Figure 239. View southeast toward the proposed tunnel alignment (beneath West Lombard Street) from Davidge Hall, University of Maryland

41. University of Maryland Law School/University College/East Hall

520 West Lombard Street

MIHP No. B-2326

The University of Maryland Law School/University College/East Hall building comprises two distinct sections, one constructed prior to 1884 and the other constructed in 1907. Located at the rear of the building, the older, one-story, four-bay, brick, Gothic Revival-style structure has a steeply pitched, slate-shingled gable roof. The newer two-and-a-half-story, brick, Classical Revival-style structure faces west toward Greene Street and Davidge Hall, and features a wide stone beltcourse, round-arched Colonial Revival-style windows, a deep wooden entablature and cornice, and a wooden parapet balustrade. The building housed the law school until 1931 and was later known as University College and East Hall. The University of Maryland Law School was determined eligible for listing in the NRHP under Criterion A for its association with the development of the University of Maryland at Baltimore campus and its transition into a modern medical campus. The building is also located within the NRHP-eligible University of Maryland Medical School – Hospital District.

All Red Line Project components would be underground in the University of Maryland Law School's vicinity; no project features would be aboveground or visible. Project work would occur beneath the University of Maryland Law School's south historic property boundary and beneath West Lombard Street.

No physical impacts to University of Maryland Law School would occur; no project activity is proposed within the property's NRHP boundary. Therefore, no effects to the property's integrity of location, design, materials, and workmanship would occur.

The University of Maryland Law School's integrity of setting is diminished due to the nearby construction of non-historic buildings and a surface parking lot, although some older buildings remain intact in its vicinity. All proposed project components would be located underground and would not alter the property's visual setting. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the property were identified. Therefore, project implementation would have no effect to the University of Maryland Law School's integrity of setting.

Furthermore, no project activity would alter the property's feeling as a late-nineteenth- and early twentieth-century institutional building or its association with the development of the University of Maryland at Baltimore's campus. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the University of Maryland Law School/University College/East Hall.

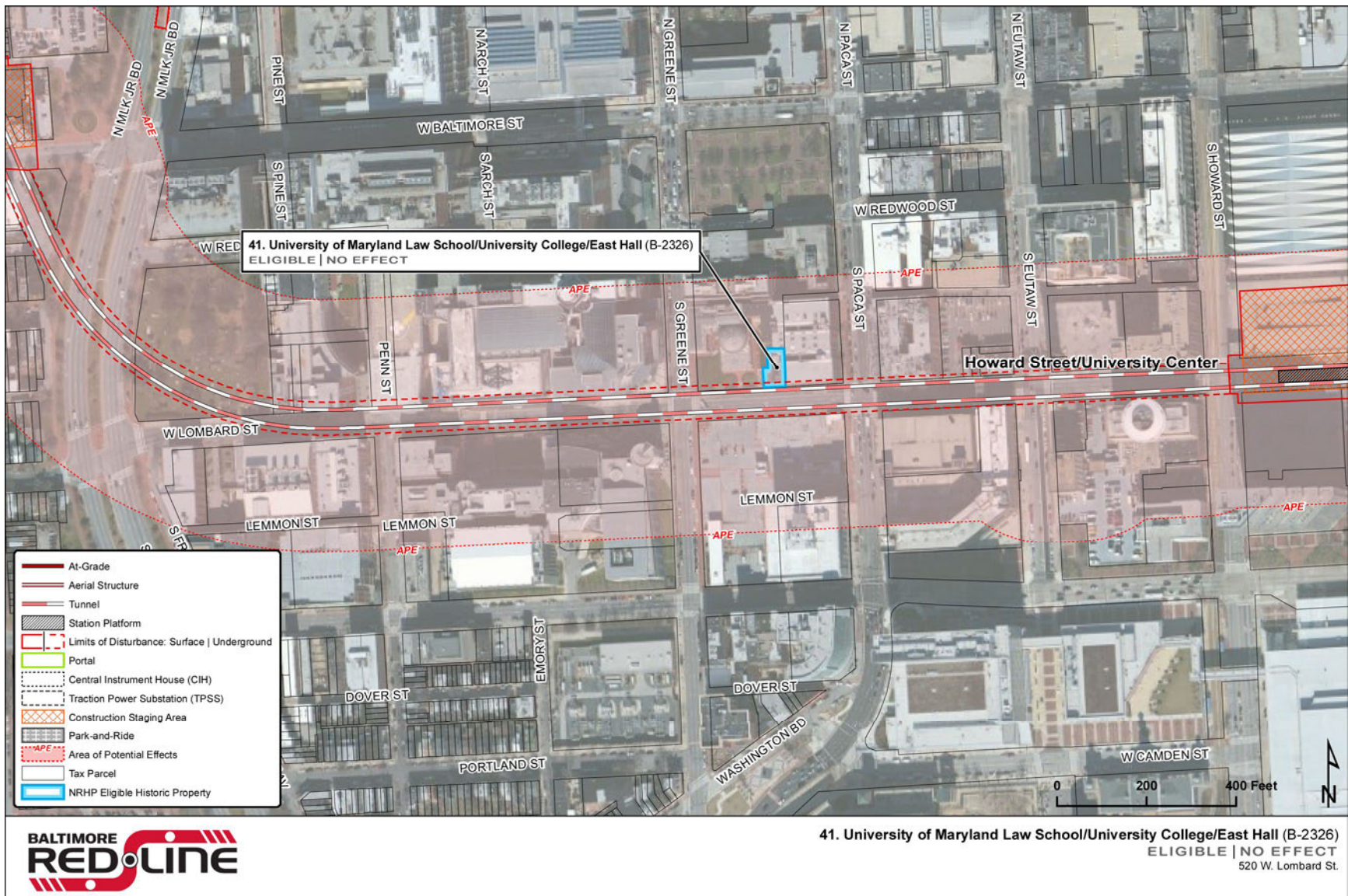


Figure 240. Proposed project in vicinity of University of Maryland Law School/University College/East Hall



Figure 241. View northeast toward the proposed tunnel alignment (beneath West Lombard Street) and to the University of Maryland Law School/University College/East Hall (see red arrow)



Figure 242. View southwest toward the proposed tunnel alignment (beneath West Lombard Street) from the University of Maryland Law School/University College/East Hall



Figure 243. View southeast toward the proposed tunnel alignment (beneath West Lombard Street); University of Maryland Law School/University College/East Hall to east (see red arrow)

42. Gray Laboratory

520 West Lombard Street

MIHP No. B-3583

Gray Laboratory is a four-story, red brick-clad, L-shaped institutional building constructed in 1894. The building's three-story, six-bay-wide, north-south section has an elevated basement and runs southward along the Inner Harbor Loft Apartments' rear west elevation. The building's four-story, four-bay-wide, east-west wing faces the Bressler Research Building's south elevation. A sign reading "Gray Laboratory" is painted below the north-south wing's third-floor windows. The building is devoid of ornamentation; many of its windows and doors have been replaced. The south and west wings frame a small, rectangular, grassy courtyard located between Davidge Hall and Gray Laboratory. Gray Laboratory was constructed on the foundations of the former Practice Hall and was named in honor of a former patient who provided a generous bequest. By the 1930s, the building was one of three major lab facilities at the University of Maryland Medical School. It is the third oldest medical school building on the University of Maryland at Baltimore's campus and the campus' oldest laboratory facility. Gray Laboratory was determined eligible for listing in the NRHP under Criterion A for its association with the development of the University of Maryland's Baltimore campus as the medical college's second building, and under Criterion C as a good example of 1890s institutional architecture. It is also located within the NRHP-eligible University of Maryland Medical School – Hospital District.

All Red Line Project components would be underground in Gray Laboratory's vicinity; no project features would be aboveground or visible. Project work would occur approximately 90 feet south of Gray Laboratory's south elevation and south historic property boundary, and beneath West Lombard Street.

No physical impacts to Gray Laboratory would occur; no project activity is proposed within the property's NRHP boundary. Therefore, no effects to the property's integrity of location, design, materials, and workmanship would occur.

Gray Laboratory retains integrity of setting. All proposed project components would be located underground and would not alter the property's visual setting. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the property were identified. Therefore, project implementation would have no effect to Gray Laboratory's integrity of setting.

Furthermore, no project activity would alter the property's feeling as an 1890s institutional building, or its association with that building type and the development of the University of Maryland's Baltimore campus. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to Gray Laboratory.

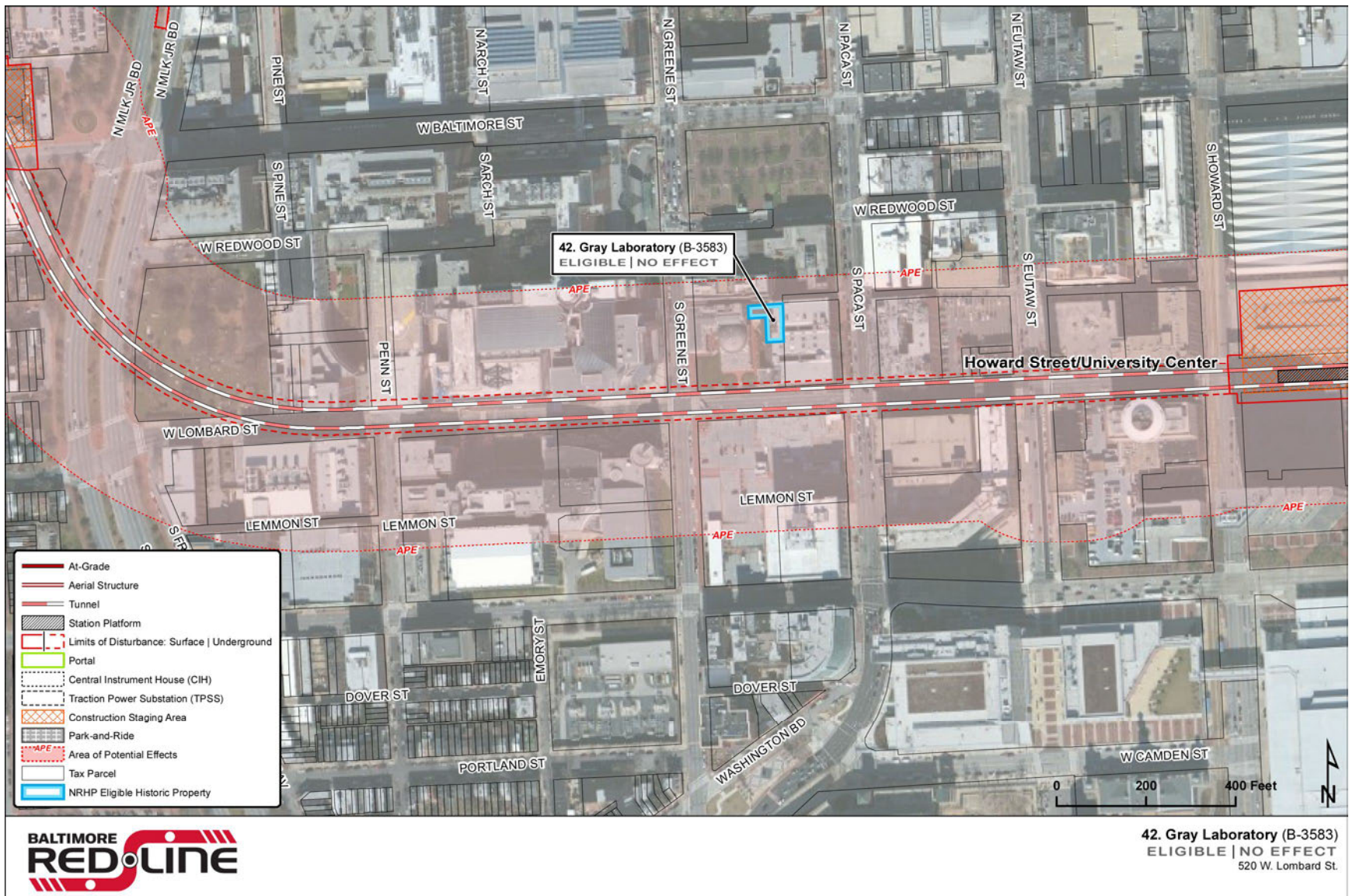


Figure 244. Proposed project in vicinity of Gray Laboratory



Figure 245. View northeast toward the proposed tunnel alignment (beneath West Lombard Street) and to Gray Laboratory (see red arrow)



Figure 246. View south toward the proposed tunnel alignment (beneath West Lombard Street) from the Gray Laboratory (in east foreground)

43. Loft Historic District South

Both sides of the 500 block of West Pratt Street, bounded by South Greene and South Paca streets

MIHP No. B-4094

The Loft Historic District South comprises seven large manufacturing buildings stylistically representative of Romanesque Revival, Neo-Classical, and Victorian era styles and early modern industrial architectural design; the buildings were constructed between 1890 and 1905. Most of the buildings are five to nine stories in height, but two are only two stories. Four buildings have been converted into a loft apartment building complex known as the Greenhouse, two have been converted into an office building complex, and one is still used for manufacturing. Architectural features include decorative brickwork, large arched openings, multi-pane industrial windows, and classical detailing. The Loft Historic District South was listed in the NRHP under Criterion A for its association with Baltimore's manufacturing industry and leading industrial firms, and under Criterion C for its collection of intact vertical manufactory buildings representing Romanesque Revival and Victorian era styles, and early modern industrial design by local architects including George Archer and Simonson & Pietsch. The district also includes the NRHP-listed Sonneborn Building (Paca-Pratt Building).

All Red Line Project components would be underground in the Loft Historic District South's vicinity; no project features would be aboveground or visible. Project work would occur approximately 185 feet north of the Loft Historic District South's north NRHP boundary, and beneath West Lombard Street.

No physical impacts to the Loft Historic District South would occur; no project activity is proposed within the district's NRHP boundary. Therefore, no effects to the district's integrity of location, design, materials, and workmanship would occur.

The Loft Historic District South retains integrity of setting, although it is diminished due to the construction of non-historic buildings and surface parking lots in its vicinity. All proposed project components would be located underground and would not alter the district's visual setting. No historically significant views to or from the district would be obscured by project implementation, and no character-defining features of the district's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the district were identified. Therefore, project implementation would have no effect to the Loft Historic District South's integrity of setting.

Furthermore, no project activity would alter the district's feeling as a collection of manufacturing buildings representative of Romanesque Revival, Neo-Classical, and Victorian-era styles and early modern industrial architectural design, or its association with those styles, building types, and Baltimore's historic manufacturing industry and leading industrial firms. Therefore, project implementation would have no effect to the district's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the Loft Historic District South.

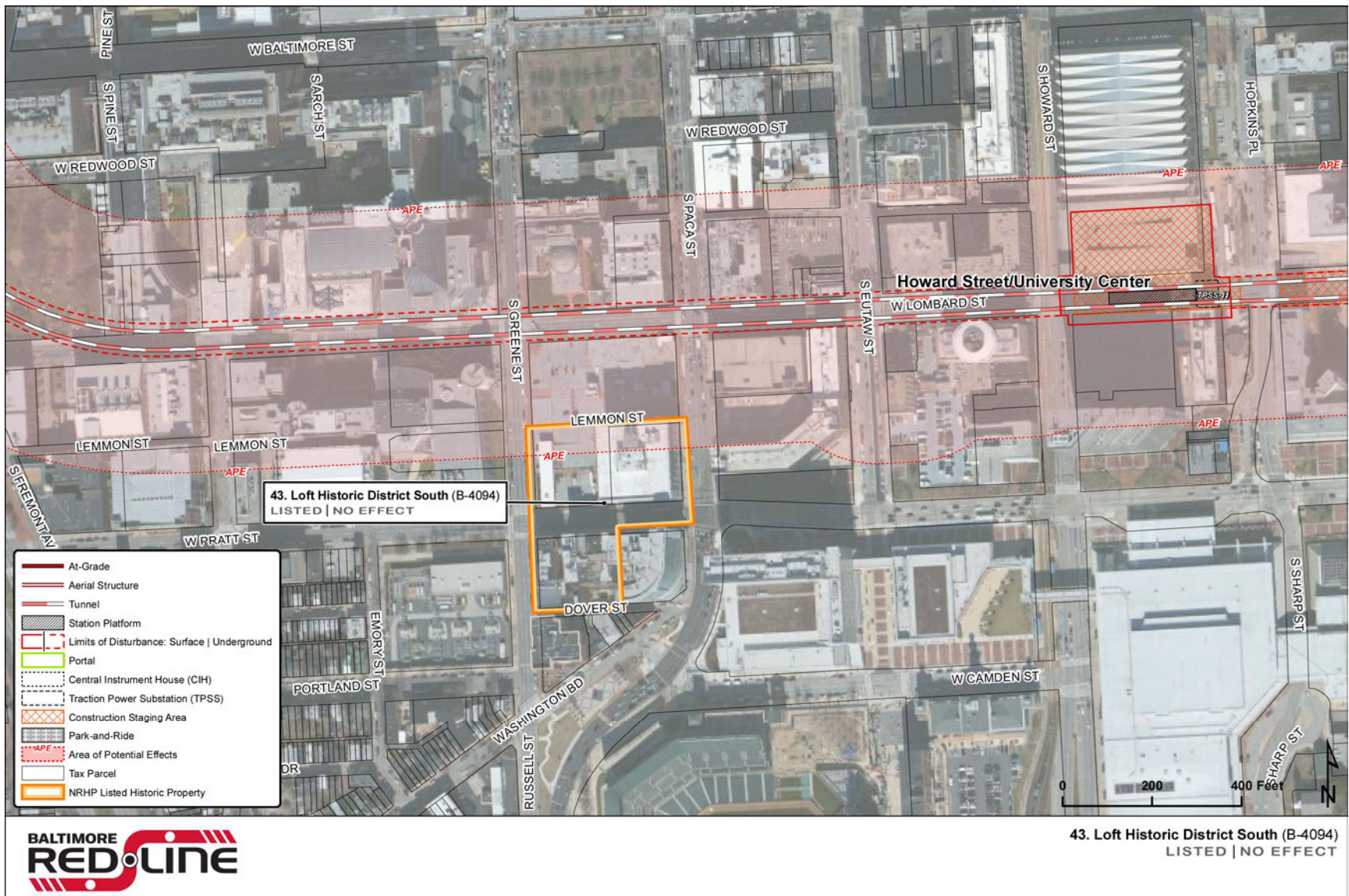


Figure 247. Proposed project in vicinity of Loft Historic District South



Figure 248. View south from the proposed tunnel alignment (beneath West Lombard Street) along South Green Street and toward the Loft Historic District South along South Greene Street



Figure 249. View north from the Loft Historic District South (along South Greene Street) and toward the proposed tunnel alignment beneath West Lombard Street



Figure 250. View southwest from the proposed tunnel alignment (beneath West Lombard Street) along South Paca Street and toward the Loft Historic District South



Figure 251. View north from the Loft Historic District South (to the west) along South Paca Street to the proposed tunnel alignment beneath West Lombard Street

44. Sonneborn Building (Paca-Pratt Building)

110 South Paca Street

MIHP No. B-2330

The Sonneborn Building (Paca-Pratt Building) is a nine-story, Neoclassical Revival-style industrial building constructed of reinforced concrete clad in brick with a coursed ashlar foundation and stone trim. Constructed in 1905 for Henry Sonneborn and Company as a vertical clothing manufactory, the Sonneborn Building was designed after the Great Baltimore Fire of 1904 and consequently had the latest in fireproof construction and sprinkler systems. It also may have been one of the earliest steel-and-concrete buildings in Baltimore. The Sonneborn Building was listed in the NRHP under Criterion A for its association with Baltimore's manufacturing industry and its association with the city's fireproof construction movement after the Great Baltimore Fire, and under Criterion C as a good example of a Neoclassical Revival-style industrial building of fireproof reinforced concrete construction designed by the firm of Otto Simonson and Theodore Wells Pietsch. The property is also a contributing building to the NRHP-listed Loft Historic District South.

All Red Line Project components would be underground in the Sonneborn Building's vicinity; no project features would be aboveground or visible. Project work would occur approximately 200 feet north of the Sonneborn Building's north elevation and north historic property boundary, and beneath West Lombard Street.

No physical impacts to the Sonneborn Building would occur; no project activity is proposed within the property's NRHP boundary. Therefore, no effects to the property's integrity of location, design, materials, and workmanship would occur.

The Sonneborn Building does not retain integrity of setting due to the construction of non-historic buildings in its vicinity. All proposed project components would be located underground and would not alter the property's visual setting. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the property were identified. Therefore, project implementation would have no effect to the Sonneborn Building's integrity of setting.

Furthermore, no project activity would alter the property's feeling as an early twentieth-century Neoclassical Revival-style industrial building, or its association with that style, building type, or Baltimore's manufacturing industry and fireproof construction movement after the Great Baltimore Fire. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the Sonneborn Building (Paca-Pratt Building).

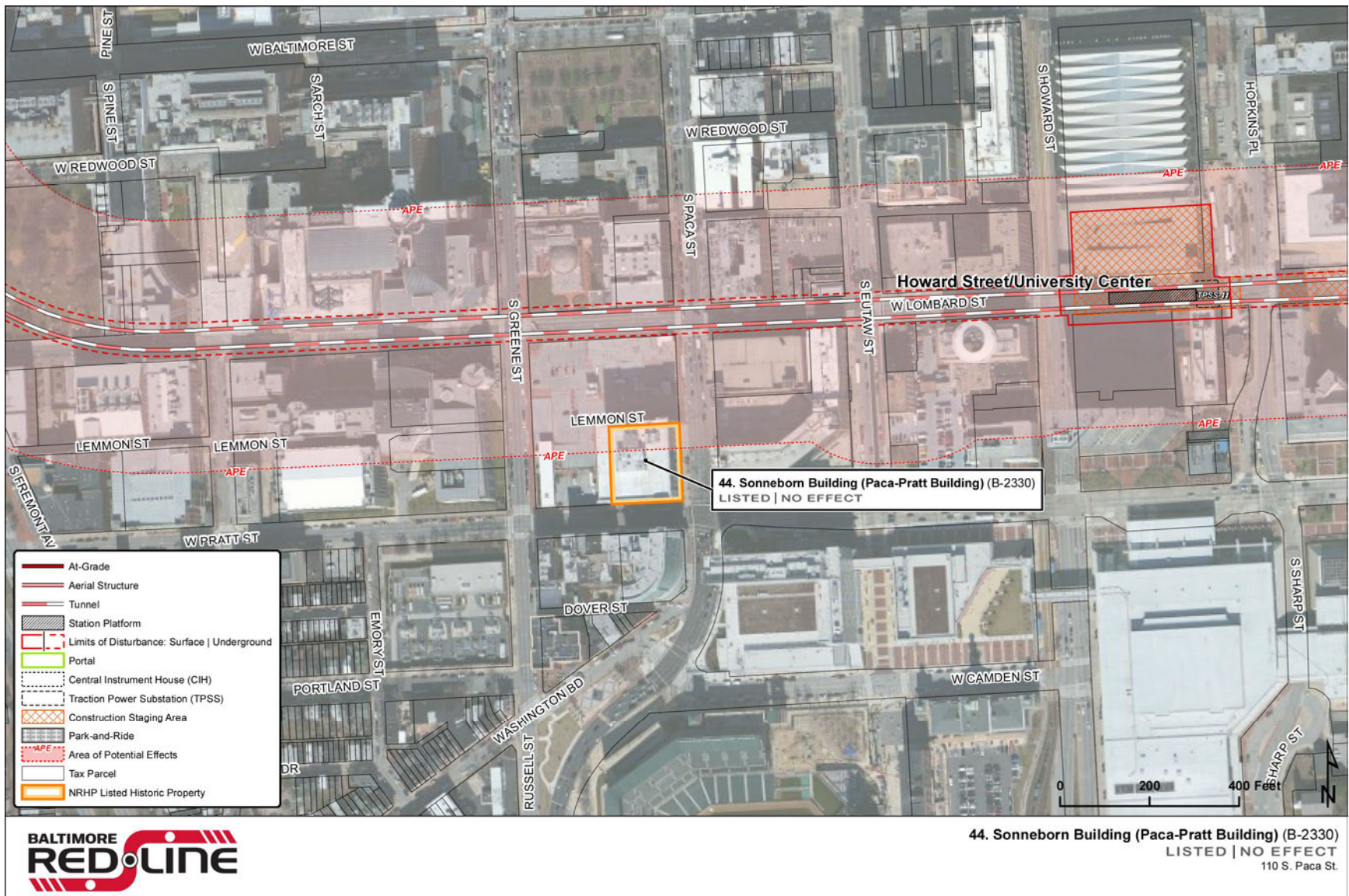


Figure 252. Proposed project in vicinity of Sonneborn Building (Paca-Pratt Building)



Figure 253. View southwest along South Paca Street to the Sonneborn Building from the proposed tunnel alignment beneath West Lombard Street



Figure 254. View north toward the proposed tunnel alignment (beneath West Lombard Street) from South Paca Street; Sonneborn Building in west foreground

45. Loft Historic District North

Paca, Redwood, and Eutaw Streets

MIHP No. B-4093

The Loft Historic District North comprises twelve large five-to-seven-story manufacturing buildings stylistically representative of Romanesque Revival and Victorian-era styles, and early modern industrial architectural design. Constructed from 1870 to 1915, most of the buildings are still used for manufacturing purposes, although a few have been converted into loft apartments or offices. Architectural features include cast iron storefronts, decorative brickwork, terra cotta ornamentation, and rooftop water tanks. The Loft Historic District North was listed in the NRHP under Criterion A for its association with Baltimore's manufacturing industry and leading industrial firms, and under Criterion C for its collection of intact manufacturing buildings representing Romanesque Revival and Victorian era styles, and early modern industrial design by local architects including George Frederick, Parker & Thomas, Charles L. Carson and Charles Cassell.

All Red Line Project components would be underground in the Loft Historic District North's vicinity; no project features would be aboveground or visible. Project work would occur along a portion of the Loft Historic District North's south NRHP boundary, and beneath West Lombard Street.

No physical impacts to the Loft Historic District North would occur; no project activity is proposed within the district's NRHP boundary. Therefore, no effects to the district's integrity of location, design, materials, and workmanship would occur.

The Loft Historic District North retains integrity of setting, although it is diminished due to the construction of non-historic buildings and surface parking lots in its vicinity. All proposed project components would be located underground and would not alter the district's visual setting. No historically significant views to or from the district would be obscured by project implementation, and no character-defining features of the district's setting would be impacted. Therefore, project implementation would have no effect to the Loft Historic District North's integrity of setting.

Furthermore, no project activity would alter the district's feeling as a collection of manufacturing buildings representative of Romanesque Revival and Victorian-era styles and early modern industrial architectural design, or its association with those styles, building types, and Baltimore's historic manufacturing industry and leading industrial firms. Therefore, project implementation would have no effect to the district's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the Loft Historic District North.

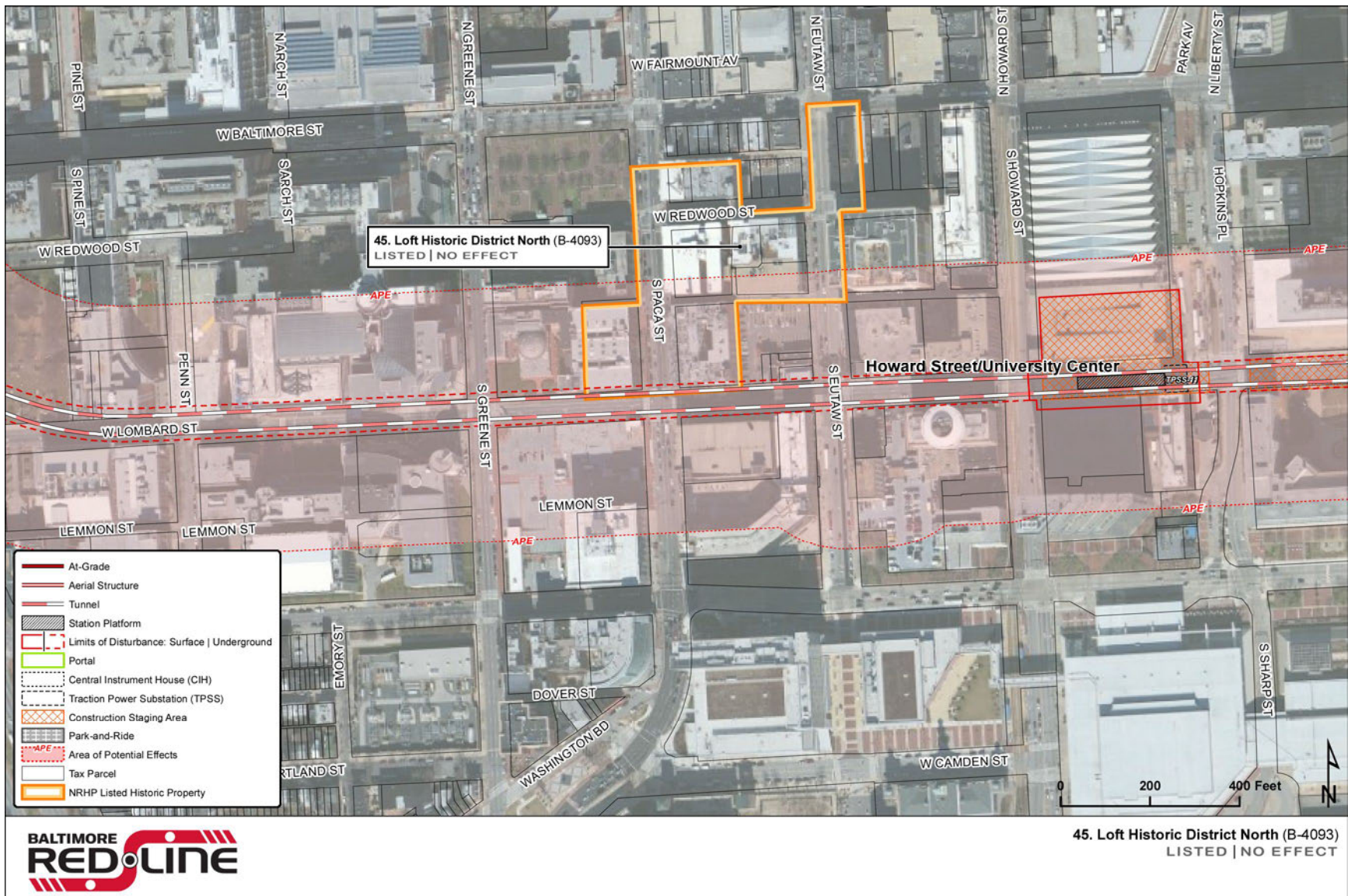


Figure 255. Proposed project in vicinity of Loft Historic District North



Figure 256. View south from within the Loft Historic District North (at South Paca Street) toward the proposed tunnel alignment beneath West Lombard Street

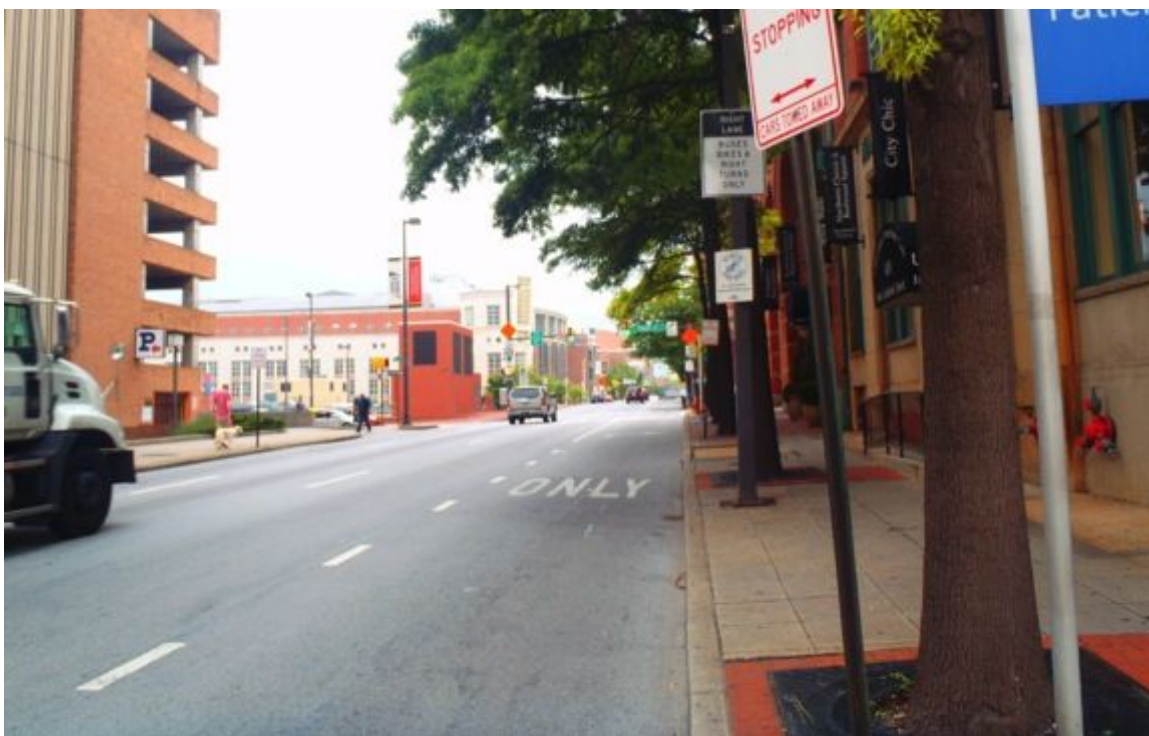


Figure 257. View west from the Loft Historic District North along the proposed tunnel alignment (beneath West Lombard Street) and to South Paca Street



Figure 258. View east along proposed tunnel alignment (beneath West Lombard Street) from South Paca Street, with the Loft Historic District North to north

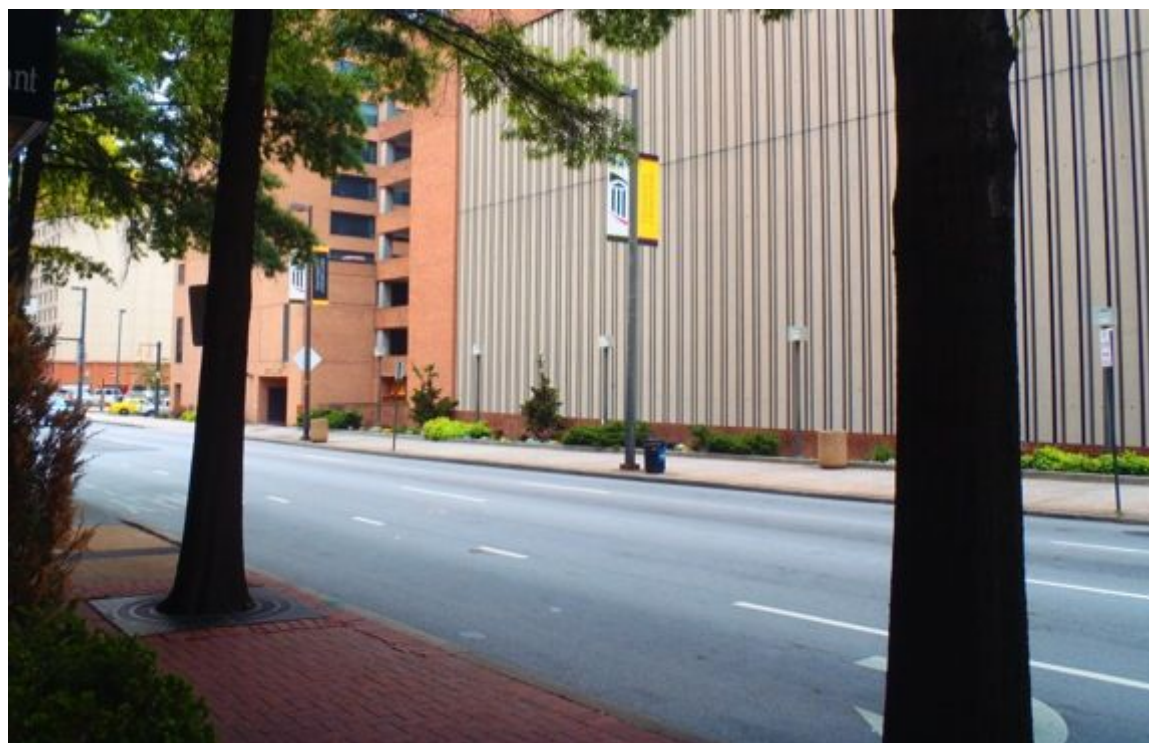


Figure 259. View southeast toward the proposed tunnel alignment (beneath West Lombard Street) from the Loft Historic District North (at South Paca Street)

46. Heiser, Rosenfeld, and Strauss Buildings (Inner Harbor Lofts I)

32-34 South Paca Street, 36-38 South Paca Street, 40-42 South Paca Street
MIHP Nos. B-2325, B-2323, B-2324

The Heiser, Rosenfeld, and Strauss Buildings (Inner Harbor Lofts I) comprises a complex of three six-story, brick, industrial buildings. The Heiser Building is a six-story, brick, stone, and iron-clad, Romanesque Revival-style factory with a showroom, constructed in 1886 by Charles Heiser, a shoe manufacturer. The Rosenfeld Building is a six-story, rusticated brick-clad, Beaux Arts-style factory building constructed in 1905 for E. Rosenfeld and Company, a pajama garment manufacturer. The Strauss Building is a six-story, brick, terra cotta, and cast iron-clad, late Victorian era factory building constructed in 1887. It was originally used as two separate facilities by the Kinny Tobacco Company and hat manufacturers M.S. Levy and Sons, and later combined as one facility in 1910 by clothing manufacturers, Strauss Brothers. The Heiser, Rosenfeld, and Strauss Buildings were listed in the NRHP under Criterion A for their association with the development of the local Baltimore clothing, pharmaceutical, and tobacco industries of the late nineteenth and early twentieth centuries, and under Criterion C as excellent examples of vertical manufactories that were concentrated in downtown Baltimore from 1850 to 1910 and executed in the Romanesque Revival and Beaux Arts styles.

All Red Line Project components would be underground in the Heiser, Rosenfeld, and Strauss Buildings' vicinity; no project features would be aboveground or visible. Project work would occur beneath the buildings' south historic property boundary and beneath West Lombard Street.

No physical impacts to the Heiser, Rosenfeld, and Strauss Buildings would occur; no project activity is proposed within the NRHP boundary. Therefore, no effects to the buildings' integrity of location, design, materials, and workmanship would occur.

The Heiser, Rosenfeld, and Strauss Building's integrity of setting is diminished by a multi-story parking garage and surface parking lot in its vicinity. All proposed project components would be located underground and would not alter the buildings' visual setting. No historically significant views to or from the buildings would be obscured by project implementation, and no character-defining features of their setting would be impacted. Because no historically significant views would be obscured, no visual effects to the buildings were identified. Therefore, project implementation would have no effect to the Heiser, Rosenfeld, and Strauss Building's integrity of setting.

Furthermore, no project activity would alter the property's feeling as a complex of late-nineteenth- and early twentieth-century industrial buildings in the Romanesque Revival and Beaux Arts styles, or its association with those styles or the development of Baltimore's clothing, pharmaceutical, and tobacco industries. Therefore, project implementation would have no effect to the buildings' integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the Heiser, Rosenfeld, and Strauss Buildings (Inner Harbor Lofts I).

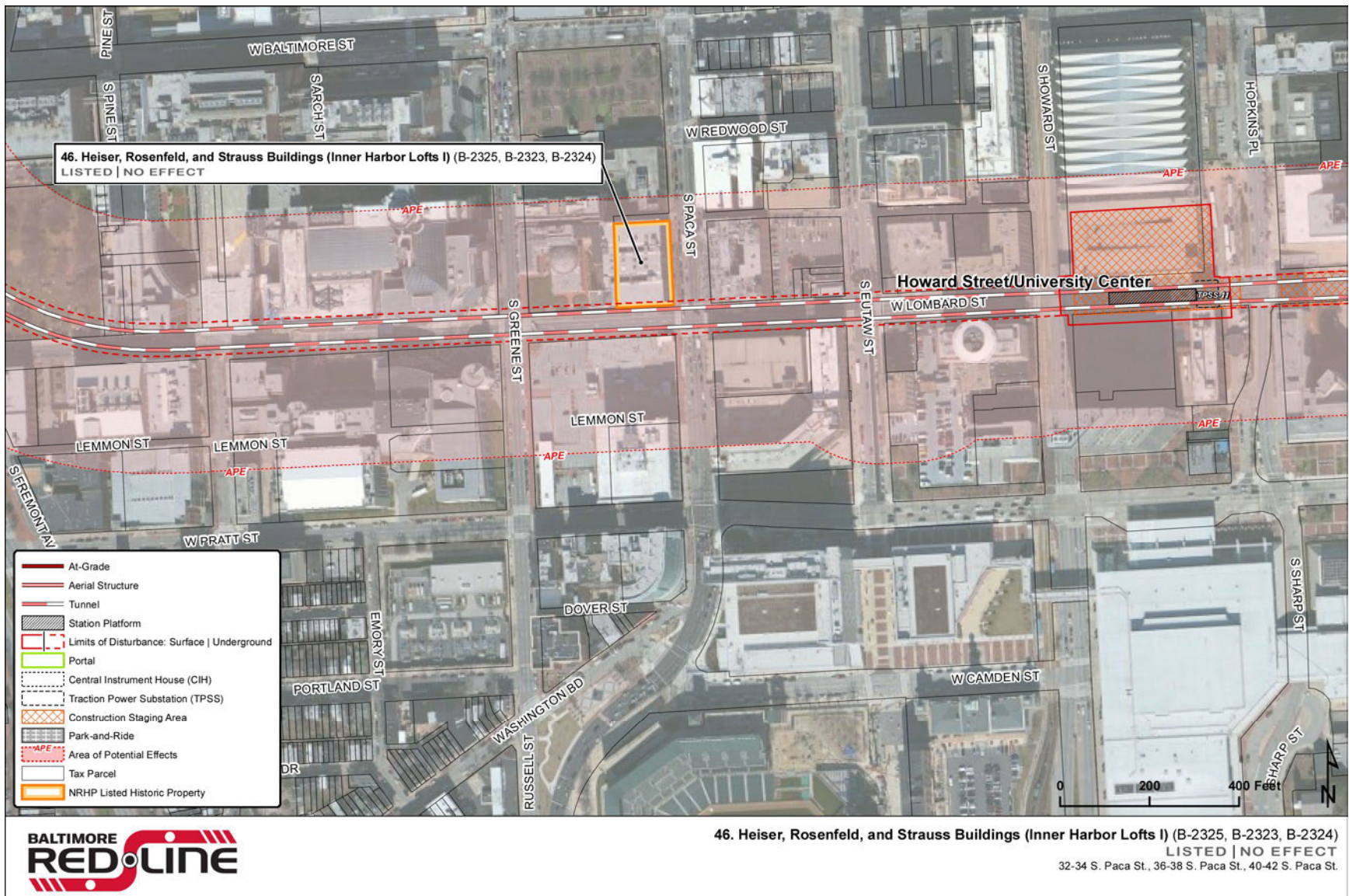


Figure 260. Proposed project in vicinity of Heiser, Rosenfeld, and Strauss Buildings (Inner Harbor Lofts I)



Figure 261. View northwest toward the proposed tunnel alignment (beneath West Lombard Street) and to the Heiser, Rosenfeld, and Strauss Buildings



Figure 262. View south along South Paca Street from the Heiser, Rosenfeld, and Strauss Buildings (on west) toward the proposed tunnel alignment beneath West Lombard Street

47. Emerson Bromo-Seltzer Tower

312-318 West Lombard Street

MIHP No. B-38

The Emerson Bromo-Seltzer Tower is a fifteen-story, brick and stone-clad, Romanesque Revival-style tower topped by a four-dial gravity clock and projecting brackets supporting an octagonal crenellated tower. Constructed in 1911 by prominent Baltimore resident Captain Isaac E. Emerson, architect Joseph Evans Sperry's design was inspired by the Palazzo Vecchio in Florence, Italy. It was the tallest building in Baltimore until the 1930s, serving as a visual landmark and symbol of the city. Until 1936, the tower originally was crowned by a giant revolving Bromo-Seltzer bottle that was illuminated at night. The Emerson Bromo-Seltzer Tower was listed in the NRHP under Criterion A for its association as a visual landmark for several generations of Baltimore citizens and as a symbol of the city to other Americans; under Criterion B for its association with Captain Isaac E. Emerson, who was instrumental in the organization of the Maryland Naval Reserves and the Emerson Drug Company; and under Criterion C as a unique example of a Romanesque Revival-style tower inspired by the Palazzo Vecchio in Florence, Italy.

The majority of the Red Line Project components would be underground in the Emerson Bromo-Seltzer Tower's vicinity; only elements of the Howard Street/University Center Station would be aboveground or visible. The Howard Street/University Center Station would be located underground approximately 420 feet east of the Emerson Bromo-Seltzer Tower and would be constructed using cut-and-cover tunneling techniques that would adjoin the alignment's bored tunnel; the station entrance would be located within an existing parking structure. Station construction would require substantial disruption on West Lombard Street and include entrance construction. Entrance features would include escalators as well as a fan plant/vent structure that would be four to six stories in height; the precise appearance of these structures, including cladding materials, has not been determined but they would be located within the garage. Construction activities are anticipated to last up to approximately four years; because of this time span, they are not considered temporary impacts. The remaining project work would occur underground and beneath the Emerson Bromo-Seltzer Tower's south historic property boundary and beneath West Lombard Street. Project planning and engineering efforts are paying close attention to the structural components of this building to ensure that no damage occurs; additional information on any historic preservation-related concerns will be included in revised reports and/or consultation.

No physical impacts to the Emerson Bromo-Seltzer Tower would occur; no project activity is proposed within the property's NRHP boundary. Therefore, no effects to the property's integrity of location, design, materials, and workmanship would occur.

The Emerson Bromo-Seltzer Tower does not retain integrity of setting due to the construction of non-historic buildings and parking lots in its vicinity. With the exception of elements of the Howard Street/University Center Station, including escalators and a fan plant/vent structure, all proposed project components would be located underground. However, based on the fifteen-story Emerson Bromo-Seltzer Tower's scale and distance from the station's aboveground elements, these components would represent a minor change to the property's altered visual

setting. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the property were identified. Therefore, project implementation would have no effect to the Emerson Bromo-Seltzer Tower's integrity of setting.

Furthermore, no project activity would alter the property's feeling as an early twentieth-century Romanesque Revival-style tower or its association that style, Captain Isaac E. Emerson, or as a visual city landmark. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the Emerson Bromo-Seltzer Tower.

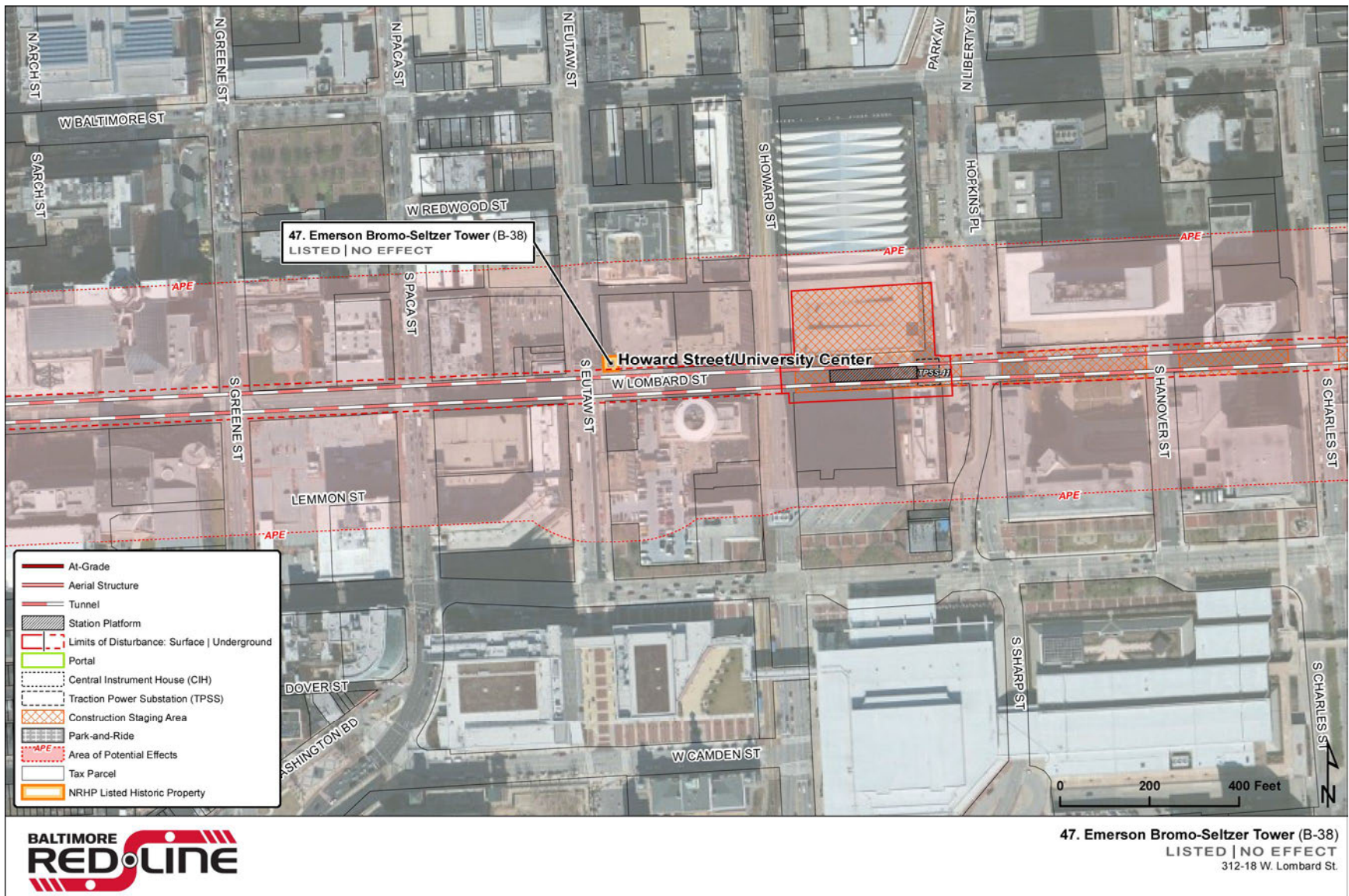


Figure 263. Proposed project in vicinity of Emerson Bromo-Seltzer Tower



Figure 264. View north to the Emerson Bromo-Seltzer Tower; proposed tunnel alignment beneath West Lombard Street in the foreground



Figure 265. View southwest from the Emerson Bromo-Seltzer Tower toward the proposed tunnel alignment beneath West Lombard Street



Figure 266. View southeast from the Emerson Bromo-Seltzer Tower toward the proposed tunnel alignment beneath West Lombard Street

48. Rombro Building

22-24 South Howard Street

MIHP No. B-2371

The Rombro Building is a six-story, brick, Queen Anne-style double warehouse building constructed in 1881. An intact cast iron storefront distinguishes the facade, which also incorporates brick, stone, terra cotta, and cast iron columns. The Rombro Building was listed in the NRHP under Criterion C as an excellent example of late Victorian-era commercial design incorporating a cast iron storefront on a double warehouse building.

The majority of the Red Line Project components would be underground in the Rombro Building's vicinity; only elements of the Howard Street/University Center Station would be aboveground or visible. The Howard Street/University Center Station would be located underground approximately 220 feet southeast of the Rombro Building and would be constructed using cut-and-cover tunneling techniques that would adjoin the alignment's bored tunnel. Station construction would require substantial disruption on West Lombard Street. Additionally, station entrance construction would occur along West Lombard Street within an existing parking structure. Entrance features would include escalators as well as a fan plant/vent structure that would be four to six stories in height; the precise appearance of these structures, including cladding materials, has not been determined but they would be located within the parking structure. Construction activities are anticipated to last for approximately four years; because of this time span, they are not considered temporary impacts. The station elements would not be visible from the building due to an intervening building that completely obstructs any views to or from the property. The remaining project work would occur underground and approximately 185 feet south of the Rombro Building's south historic property boundary, and beneath West Lombard Street.

No physical impacts to the Rombro Building would occur; no project activity is proposed within the property's NRHP boundary. Therefore, no effects to the property's integrity of location, design, materials, and workmanship would occur.

The Rombro Building does not retain integrity of setting due to the construction of non-historic buildings and parking garages in its vicinity. With the exception of elements of the Howard Street/University Center Station, including escalators and a fan plant/vent structure, all proposed project components would be located underground and would not alter the property's visual setting. The Howard Street/University Center Station's aboveground elements would not be visible from the Rombro Building due to intervening buildings, which completely screen any views to or from the historic building's east and south elevations and the proposed station. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the property were identified. Therefore, project implementation would have no effect to the Rombro Building's integrity of setting.

Furthermore, no project activity would alter the property's feeling as a late nineteenth-century, Victorian-era double warehouse building with a cast iron storefront or its association with that

building type. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the Rombro Building.

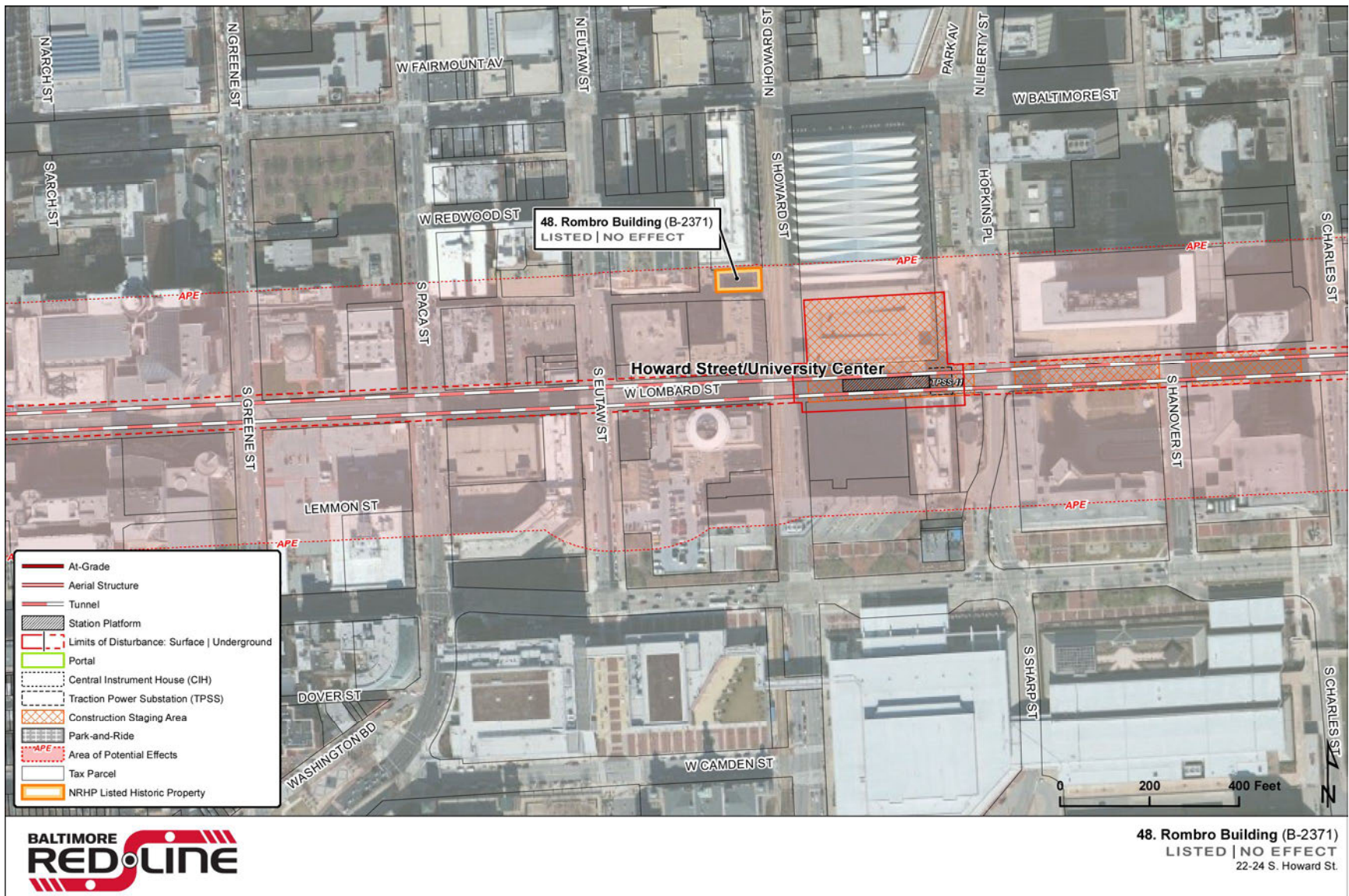


Figure 267. Proposed project in vicinity of Rombro Building



Figure 268. View northwest along South Howard Street toward the Rombro Building (see red arrow); proposed tunnel alignment in the foreground (beneath West Lombard Street)

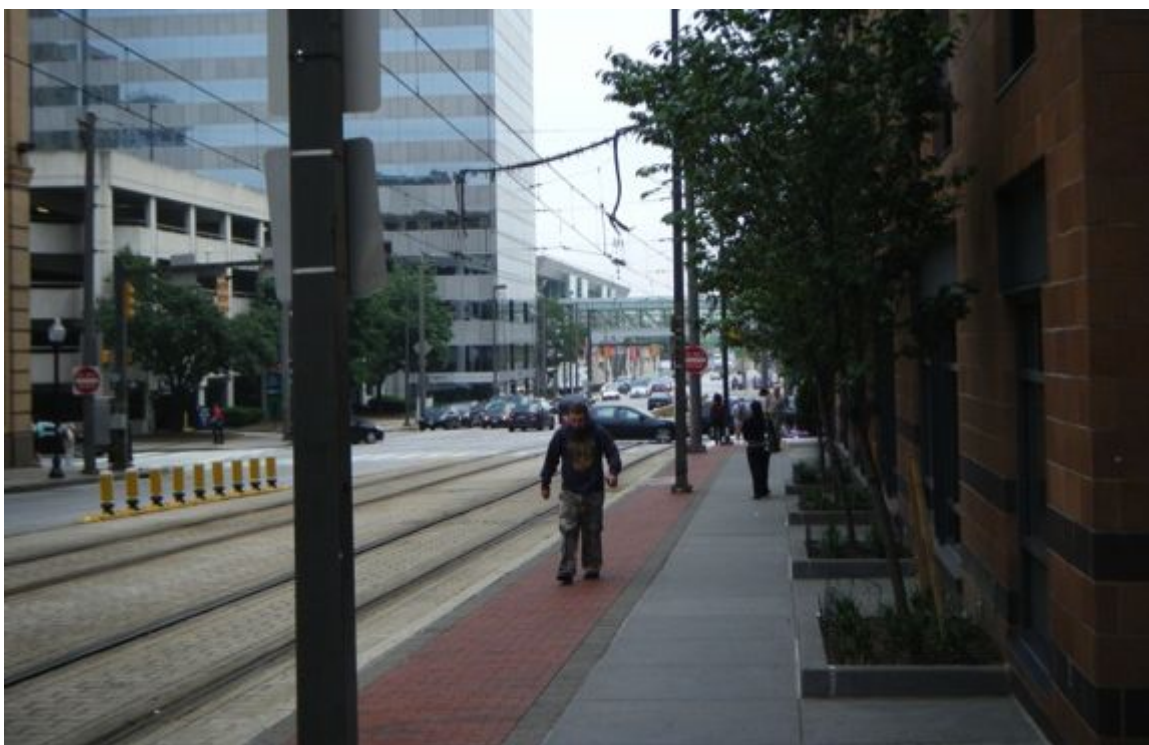


Figure 269. View southeast along South Howard Street from the Rombro Building toward the proposed tunnel alignment (beneath West Lombard Street)

49. Wilkens-Robins Building

308-314 West Pratt Street

MIHP No. B-3598

The Wilkens-Robins Building is a five-story, six-bay, brick-clad office and warehouse building with a cast iron facade; it was constructed in 1871. An intact cast iron storefront distinguishes the facade, which also incorporates brick, stone, terra cotta, and cast iron columns. The Wilkens-Robins Building was listed in the NRHP under Criterion A for its association with the development of Baltimore into an important center of cast iron construction in the post-Civil War era, and under Criterion C as one of the few remaining cast iron-fronted buildings in Baltimore and as an excellent example of evolving building technology.

The majority of the Red Line Project components would be underground in the Wilkens-Robins Building's vicinity; only elements of the Howard Street/University Center Station would be aboveground or visible. The Howard Street/University Center Station would be located underground approximately 310 feet northeast of the Wilkens-Robins Building and would be constructed using cut-and-cover tunneling techniques that would adjoin the alignment's bored tunnel. Station construction would require substantial disruption on West Lombard Street. Additionally, station entrance construction would occur along West Lombard Street within an existing parking structure. Entrance features would include escalators as well as a fan plant/vent structure that would be four to six stories in height; the precise appearance of these structures, including cladding materials, has not been determined but they would be within the parking garage. Construction activities are anticipated to last for approximately four years; because of this time span, they are not considered temporary impacts. The station elements would not be visible from the Wilkens-Robins Building due to intervening buildings that completely screen any views to or from the property. The remaining project work would occur underground and approximately 240 feet north of the Wilkens-Robins Building's north elevation and north historic property boundary, and beneath West Lombard Street.

No physical impacts to the Wilkens-Robins Building would occur; no project activity is proposed within the property's NRHP boundary. Therefore, no effects to the property's integrity of location, design, materials, and workmanship would occur.

The Wilkens-Robins Building does not retain integrity of setting due to the construction of non-historic buildings in its vicinity. With the exception of elements of the Howard Street/University Center Station, including escalators and a fan plant/vent structure, all proposed project components would be located underground. The Howard Street/University Center Station's aboveground elements would not be visible from the Wilkens-Robins Building due to intervening buildings, which completely screen views to or from the historic building's east and north elevations and the proposed station. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the property were identified. Therefore, project implementation would have no effect to the Wilkens-Robins Building's integrity of setting.

Furthermore, no project activity would alter the property's feeling as a post-Civil War cast iron-fronted building or its association with that building type and evolving, nineteenth-century building technology. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the Wilkens-Robins Building.

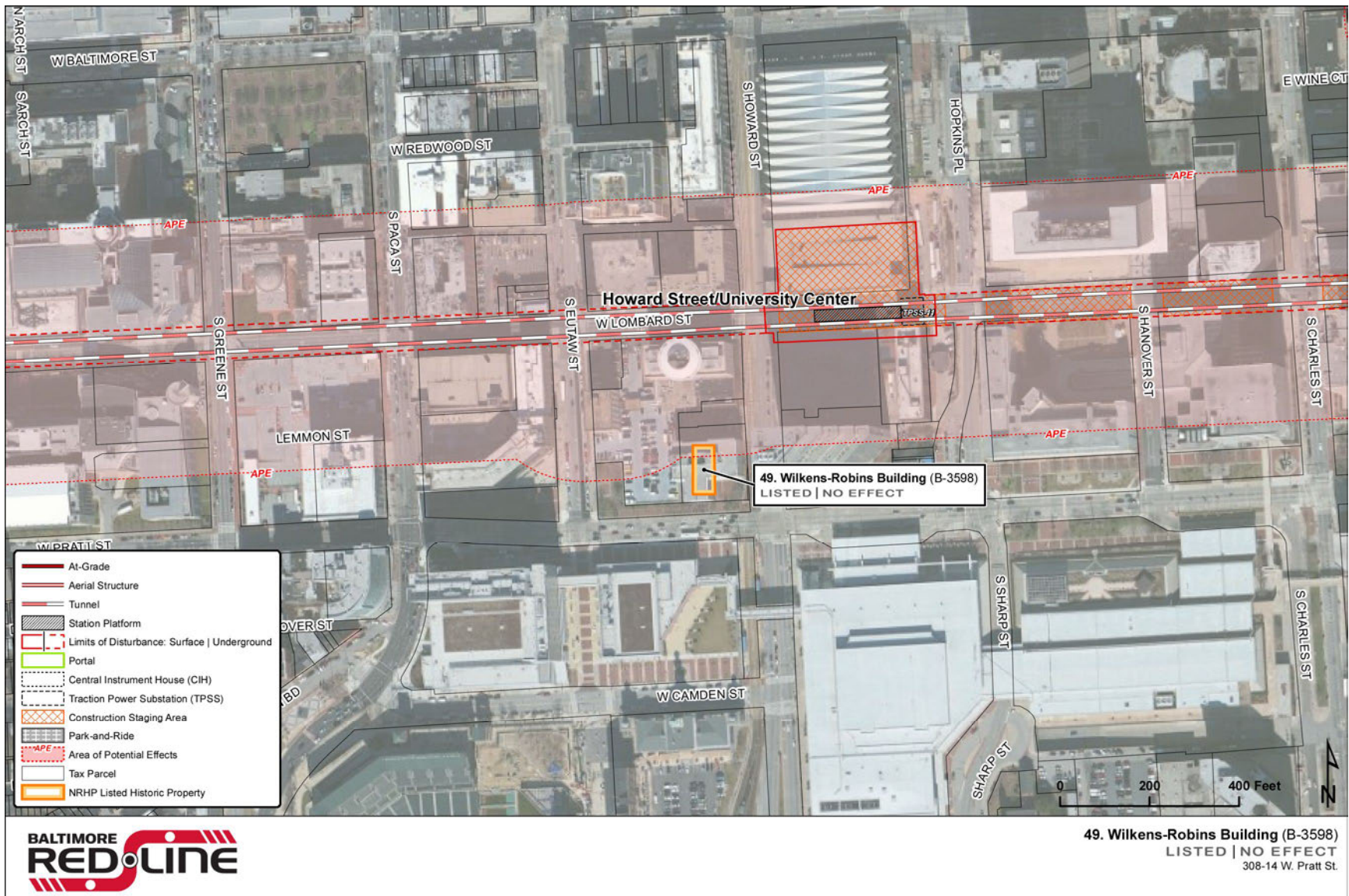


Figure 270. Proposed project in vicinity of Wilkens-Robins Building



Figure 271. View southwest along South Howard Street toward the Wilkens-Robins Building addition (see red arrow) from proposed tunnel alignment beneath West Lombard Street (in foreground)

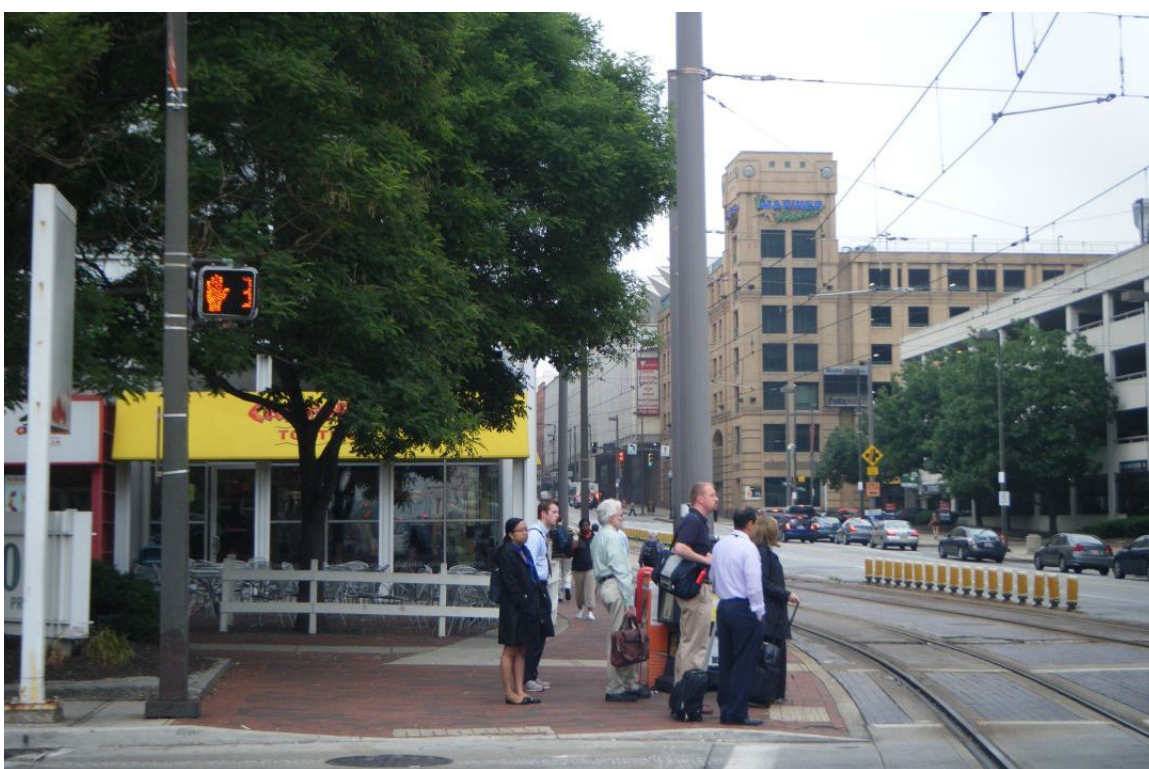


Figure 272. View north along South Howard Street toward the proposed tunnel alignment (beneath West Lombard Street); Wilkens-Robins Building addition in west foreground



Figure 273. View northeast from West Pratt Street to the original Wilkens-Robins Building; the proposed Howard Street/University Center Station and tunnel alignment (beneath West Lombard Street) located beyond

50. Howard Street Tunnel

Beneath Howard Street between Camden Station and Mount Royal Station
MIHP No. B-79

The Howard Street Tunnel is a railway tunnel located beneath Howard Street that extends from Camden Station to Mount Royal Station; it was constructed between 1890 and 1895. The tunnel's dimensions are 7,341 feet long, 21 feet high, and 39 feet wide; it is 50 to 65 feet below grade, with the exception of transitions to street level. Tunnel walls are primarily clad in brick with smaller segments constructed of stone. Portals are also constructed of cut stone. The Howard Street Tunnel was listed in the NRHP in 1973. The tunnel is significant under Criterion A for its association with Baltimore's transportation history as the industrial city required increased services. It is also significant under Criterion C as an outstanding and monumental example of engineering technology for its time period. The Howard Street Tunnel's historic property boundary includes its below-grade footprint beneath Howard Street and encompasses all built features of the tunnel, including the portals.

Red Line Project activity near the Howard Street Tunnel would be extensive due to the presence of the Howard Street/University Center Station, which is below ground. The historic tunnel is also below ground in the areas proximate to the project; both historic portals are outside the project's APE. The project alignment, located beneath West Lombard Street on an east-west axis, would also run beneath the Howard Street Tunnel, which is on a north-south axis. However, no work would occur within the tunnel's historic boundary; all work would occur beneath the existing historic tunnel. The Howard Street/University Center Station would be built directly east of the tunnel. The station would be constructed using cut-and-cover tunneling techniques that would adjoin the project alignment's bored tunnel that will pass beneath the Howard Street Tunnel. Station construction would require substantial disruption on West Lombard Street to the tunnel's east. Station entrances and a plant/vent structure of four to six stories in height would be incorporated into the 1st Mariner Arena Parking Garage. Construction activities are anticipated to last for approximately four years; because of this time span, they are not considered temporary impacts. However, because the Howard Street Tunnel is below grade at this area, it would not be impacted.

No physical impacts to the Howard Street Tunnel would occur; no project activity is proposed within the property's NRHP boundary, although the project alignment would run beneath the tunnel. Therefore, no effects to the property's integrity of location, design, materials, or workmanship would occur.

The Howard Street Tunnel retains integrity of setting. The tunnel's above-ground portals are not within the project's APE; only below-ground tunnel segments are within the APE. The project alignment extending beneath the tunnel would not affect its integrity of setting. Therefore, project elements would have no effect on the Howard Street Tunnel's integrity of setting, which is unique because it is primarily a subsurface built historic property.

The Howard Street Tunnel retains integrity of feeling and association. Character-defining features that convey the tunnel's engineering technology and period in time, as well as its association with transportation in Baltimore, are present and would not be affected by the Red

Line Project; within the project's APE, the tunnel is entirely underground. Therefore, the project would have no effect on the Howard Street Tunnel's integrity of feeling and association.

Based on this evaluation, the Red Line Project would have **no effect** to the Howard Street Tunnel.

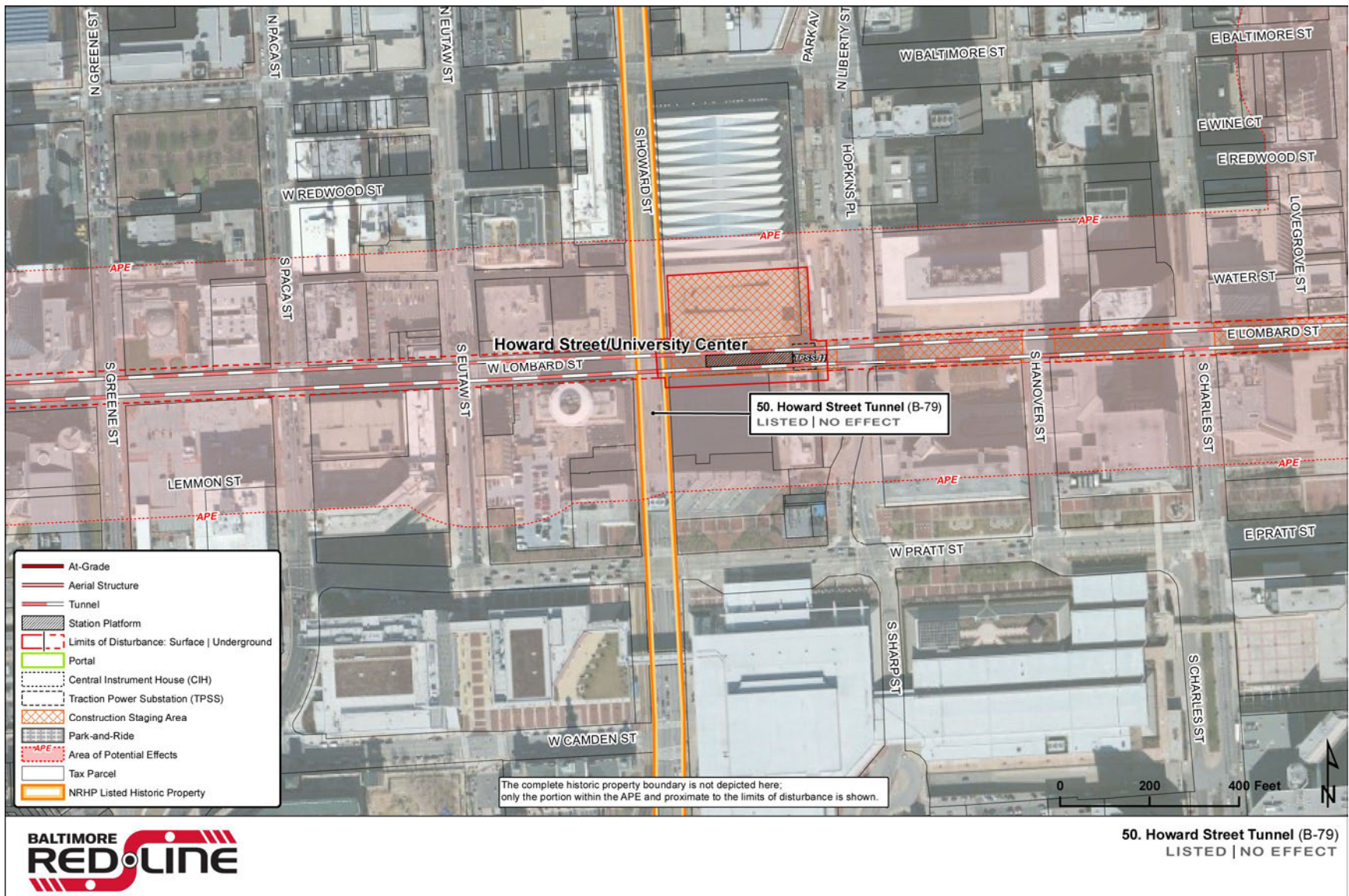


Figure 274. Proposed project in vicinity of the Howard Street Tunnel

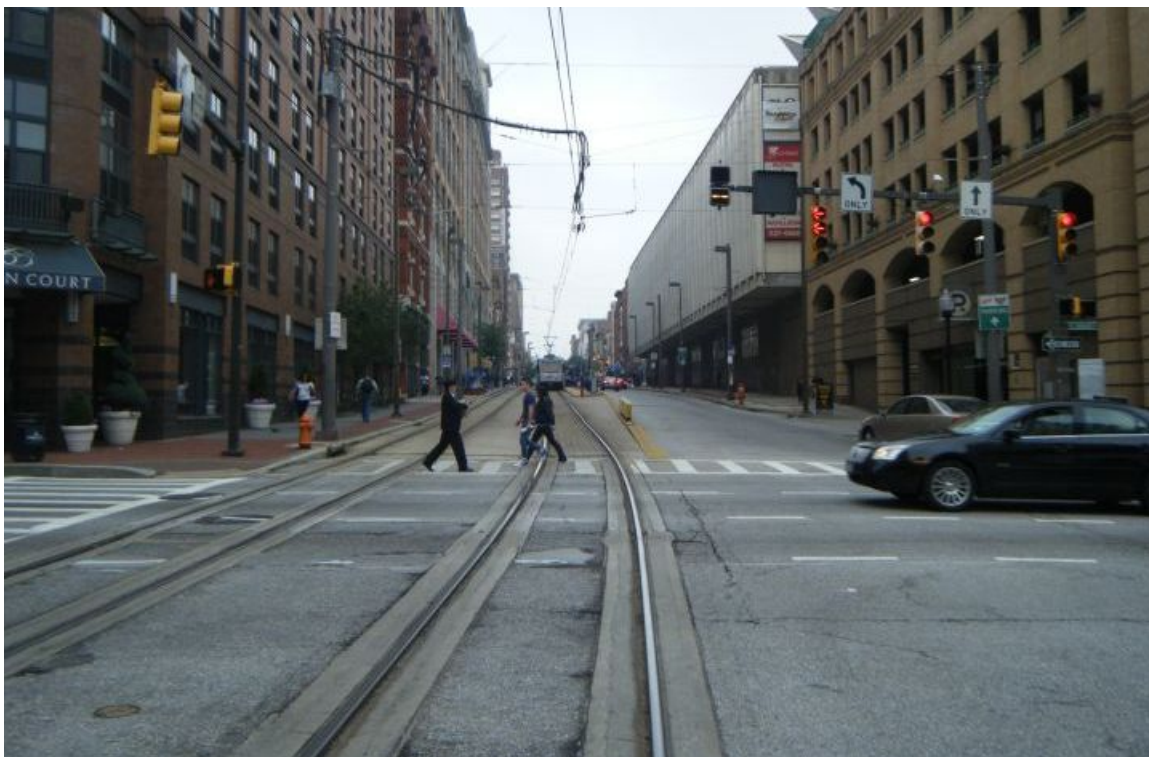


Figure 275. View north along South Howard Street above Howard Street Tunnel; proposed Red Line tunnel alignment in foreground beneath West Lombard Street



Figure 276. View south along South Howard Street above Howard Street Tunnel; proposed Red Line tunnel alignment in foreground beneath West Lombard Street

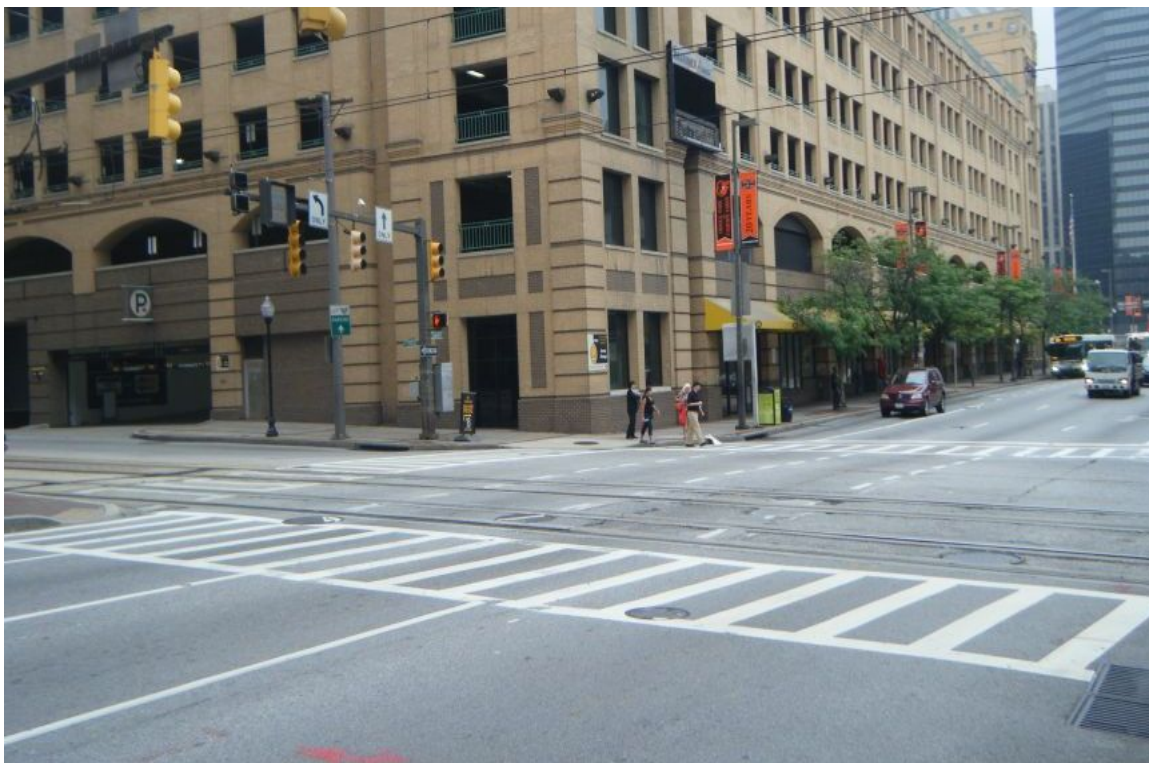


Figure 277. View northeast at South Howard Street toward the proposed Red Line tunnel alignment, the Howard Street/University Center Station, and a traction power substation (all beneath West Lombard Street), and to the Howard Street Tunnel; station entrance would be within the garage building

51. Business and Government Historic District

An irregularly shaped district located in the Inner Harbor area north of Lombard Street; see map for historic district boundaries

MIHP No. B-3935

The Business and Government Historic District contains a unified collection of commercial and municipal buildings that date from the early nineteenth century to the present day; the buildings represent Baltimore's prominent economic, commercial, and political status during this period. Within the district, many structures date to the first quarter of the twentieth century, built after the devastating downtown fire of 1904. Buildings generally are classically inspired in their design and ornamentation, with symmetrical facades topped by prominent cornices. Building materials are primarily brick or smooth stone. The Business and Government Historic District was listed in the NRHP in 1987. It is significant under Criterion A for its association with Baltimore's progress and recovery from the 1904 fire; it is also significant under Criterion C for the monumental architectural with classical references that is omnipresent within the district.

Proposed Red Line Project activities within the Business and Government Historic District include construction of the below-ground Inner Harbor station, which would require the demolition of two contributing buildings at 108-112 and 114 East Lombard Street within the north block of East Lombard Street between Calvert and Light streets; the proposed station's fan plant/vent structure would be erected at this location. These contributing buildings are within the historic district's period of significance and retain character-defining architectural features and high levels of integrity despite some condition issues; therefore, the buildings convey the historically significant themes identified for the Business and Government Historic District, including Baltimore's post-1904 development and architectural design.

The Inner Harbor Station would be below ground and beneath East Lombard Street at the district's south NRHP boundary; however, the station would be constructed using cut-and-cover methods. This construction would occur over a span of approximately four years and connect the station with bored sections of the proposed alignment's tunnel. Because the station would be below ground, a four to six story fan plant/vent structure would be constructed within the historic district and at the location of the two contributing buildings mentioned above; this structure's footprint would be 50 feet by 50 feet. Also under consideration is a below-ground pedestrian tunnel that would connect the Red Line to the Baltimore Metro subway. The remainder of proposed project activity in this area consists of underground alignment beneath East Lombard Street and along and beneath the district's south NRHP boundary.

In an effort to avoid or minimize the potential for adverse effects, other properties were considered for demolition; however, numerous contributing buildings within the Business and Government Historic District exist in the vicinity of the Red Line. Along the northern block of Lombard Street between Calvert and Light streets, all buildings are contributing, with the exception of the Brookshire Suites hotel building; also studied was the building at 36 Light Street, which is also contributing. All alternatives that met the project objectives required the demolition of contributing buildings within the historic district.

Project activity would have no effect on the Business and Government Historic District's location. However, demolition of the two contributing buildings would have an adverse effect to the district's design, materials, and workmanship.

The historic setting of the Business and Government Historic District would be altered by the extensive and prolonged station construction described above, which cannot be considered a temporary impact, as well as by the large fan plant/vent structure. The construction impacts and the fan plant/vent structure would introduce non-historic elements incompatible with the district's intact historic setting and would result in visual adverse effects to the historic district.

Although project activity is substantial, the proposed work would not permanently alter the large district's feeling as a historic center for commercial and municipal activities or its association with those activities. Therefore, project implementation would have no adverse effect to the district's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have an **adverse effect** to the Business and Government Historic District. This evaluation will be reviewed and may be revised as new project information becomes available and is confirmed.

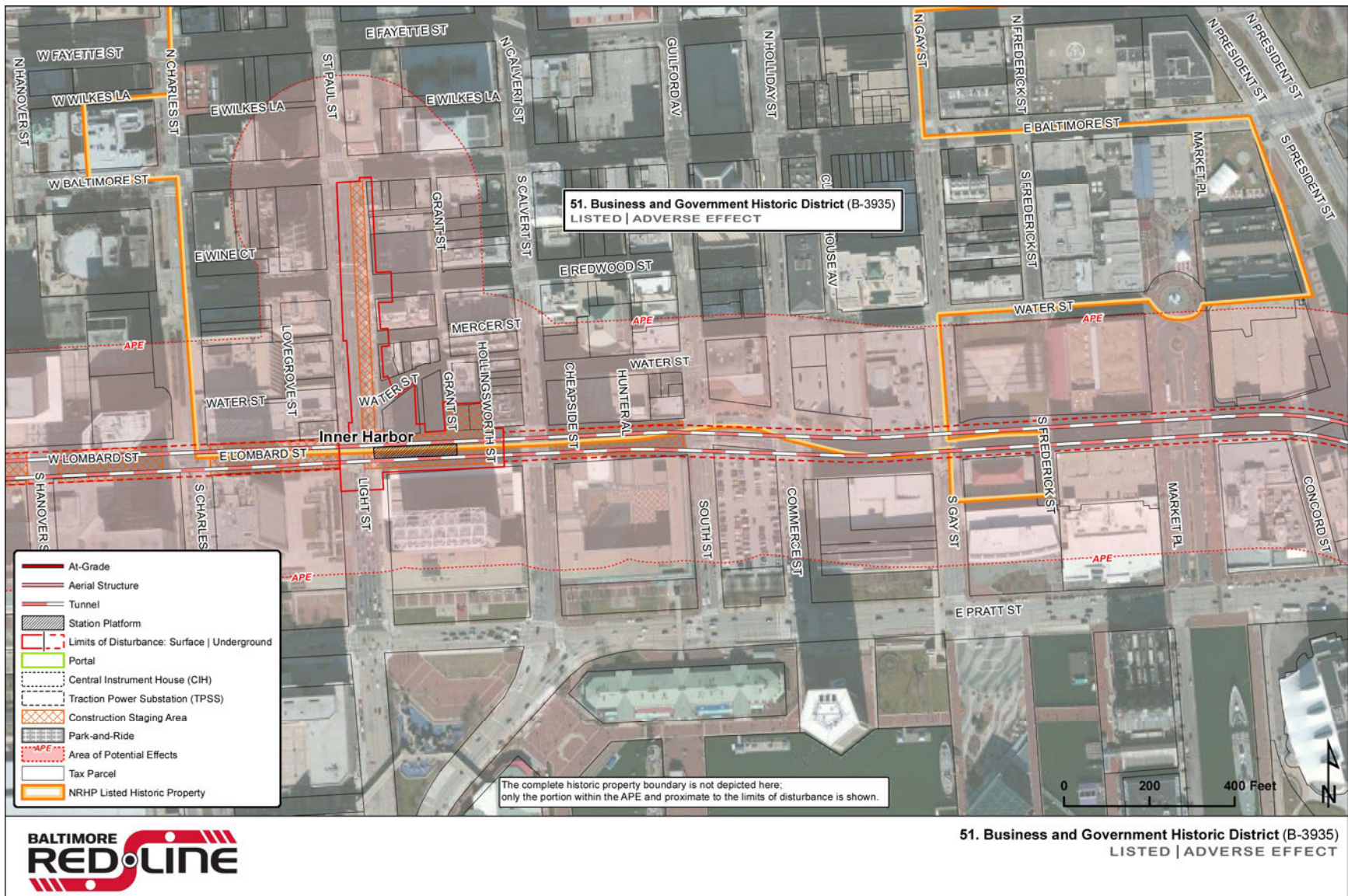


Figure 278. Proposed project in vicinity of the Business and Government Historic District



Figure 279. View southwest toward the proposed tunnel alignment (beneath East Lombard Street) from the Business and Government Historic District at Light Street



Figure 280. View south from within the Business and Government Historic District along proposed pedestrian tunnel to Metro Charles Center East (beneath Light Street); proposed Red Line tunnel alignment in background beneath East Lombard Street



Figure 281. View northeast from Light Street toward the Business and Government Historic District, and to the proposed tunnel alignment and Inner Harbor Station (both beneath East Lombard Street)



Figure 282. View southeast toward the proposed tunnel alignment and Inner Harbor Station (both beneath East Lombard Street) from the Business and Government Historic District at Light Street



Figure 283. View northwest from Calvert Street toward the Business and Government Historic District, and to the proposed tunnel alignment and Inner Harbor Station (both beneath East Lombard Street)



Figure 284. View north toward the Business and Government Historic District at Calvert Street, with the proposed tunnel alignment in foreground beneath East Lombard Street

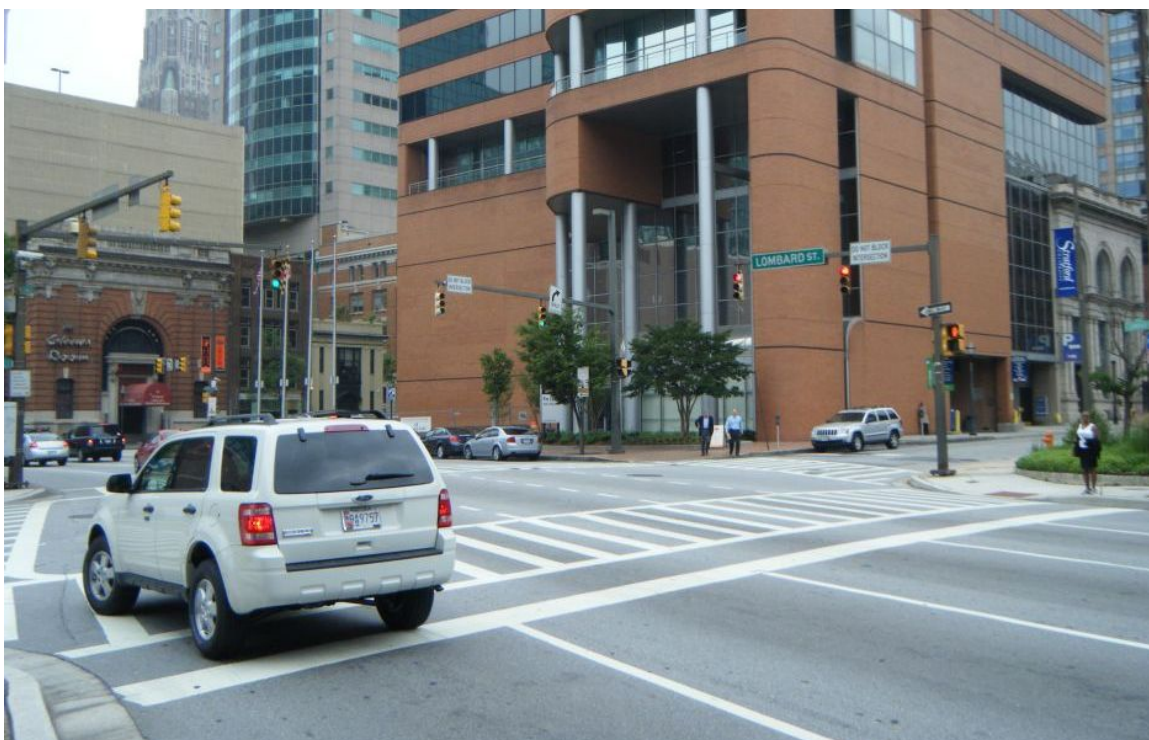


Figure 285. View northwest toward the Business and Government Historic District (at Commerce Street) and to the proposed tunnel alignment beneath East Lombard Street



Figure 286. View east from Commerce Street along proposed tunnel alignment (beneath East Lombard Street); Business and Government Historic District to north



Figure 287. View west from South Frederick Street along proposed tunnel alignment (beneath East Lombard Street) and the Business and Government Historic District

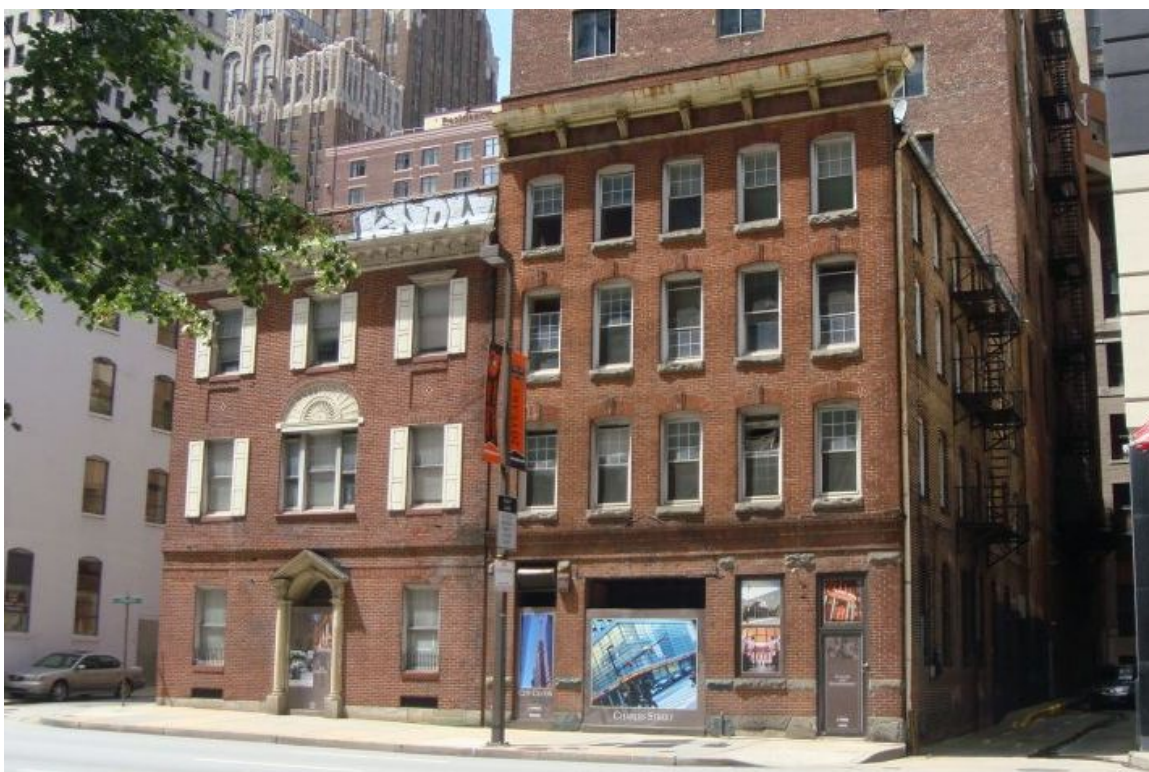


Figure 288. View north toward Business and Government Historic District contributing buildings (108-12 and 114 East Lombard Streets) that would be demolished for proposed Inner Harbor Station construction

52. Alex Brown Building

135 East Baltimore Street
MIHP No. B-117

The Alex Brown Building is a two-story, brick and marble building exhibiting the Beaux Arts style. Constructed in 1901 as a “fireproof” building, it survived the 1904 Great Baltimore Fire and contains a large, Art Nouveau, stained-glass dome at its center and decorative balconies on its exterior. The building has been the home of Alex, Brown & Sons, an investment firm established in Baltimore in 1800, since its construction. The Alex Brown Building was listed in the NRHP under Criterion A for its association with the Great Baltimore Fire and early Baltimore commerce, and under Criterion C for its high Beaux Arts style with Art Nouveau details.

Red Line Project components would be underground in the vicinity of the Alex Brown Building. Underground project work would occur beneath Light Street approximately 200 feet west of the Alex Brown Building. No proposed stations are proximate to the property.

No physical impacts to the Alex Brown Building would occur; no project activity is proposed within the property’s NRHP boundary. Therefore, no effects to the property’s integrity of location, design, materials, and workmanship would occur.

Project implementation would not affect the Alex Brown Building’s setting as all project components would be located underground and would not be visible. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property’s setting would be impacted. Because no historically significant views would be obscured, no visual effects to the property were identified. Therefore, project implementation would have no effect to the property’s integrity of setting.

Furthermore, no project activity would alter the property’s feeling as a Beaux Arts-style building with Art Nouveau details, or its association with these styles, the 1904 Great Baltimore Fire, or early commerce in Baltimore.

Based on this evaluation, the Red line Project would have **no effect** to the Alex Brown Building.

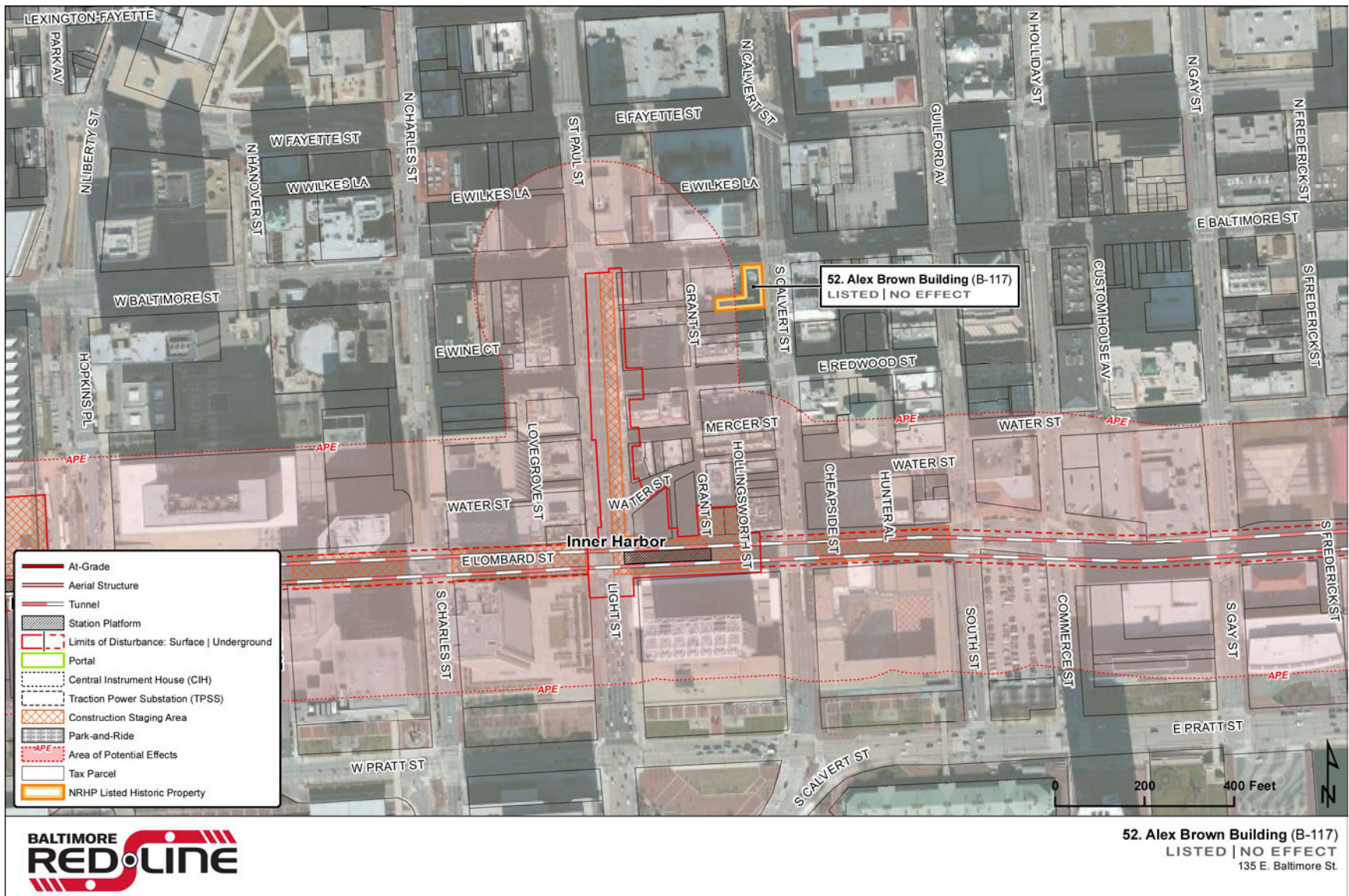


Figure 289. Proposed project in vicinity of the Alex Brown Building



Figure 290. View northwest along South Calvert Street toward the Alex Brown Building (see red arrow) from proposed tunnel alignment beneath East Lombard Street



Figure 291. View south from the Alex Brown Building (in west foreground) toward the proposed tunnel alignment beneath East Lombard Street



Figure 292. View west along East Baltimore Street from the Alex Brown Building (in south foreground) toward the proposed pedestrian tunnel to Metro Charles Center East (beneath Light Street)



Figure 293. View east along East Baltimore Street from the proposed pedestrian tunnel to Metro Charles Center East (beneath Light Street) toward the Alex Brown Building (see red arrow)

53. Canton House

300 Water Street

MIHP No. B-3705

The Canton House is a four-and-a-half-story, Colonial Revival-style building clad in marble at the first story and brick on the remaining floors. The seven-bay-wide facade has a main entrance distinguished by a pilastered architrave two fluted Corinthian columns, and a plain marble lintel; the three-bay wide west, side elevation contains a similar entrance. Marble stringcourses extend around the facade and west, side elevation above the first and fourth stories, and two marble circles at the facade's fourth story contain the building's 1923 construction date and the 1828 date that the Canton Company was founded. A gabled, brick parapet caps the facade and contains a semi-circular window, while the west, side elevation contains two gabled dormers. The Canton House was constructed as the headquarters of the Canton Company, which was established in 1828 by Peter Cooper, inventor and manufacturer of the Tom Thumb steam locomotive, and Ames Benney, a Boston entrepreneur. The company purchased 6,000 acres of land in Baltimore to serve as its pier and service area, turning it into a large manufacturing center for the Baltimore area and a center of import and export traffic. The Canton House was listed in the NRHP under Criterion A for its association with the establishment and development of Baltimore's Canton area by the Canton Company into an import and export center and the development of Baltimore into a large and important manufacturing center. It was also listed under Criterion C as a good example of a Colonial Revival-style building designed to reflect the company's Colonial period origins. The building is also located in the Business and Government Historic District.

All Red Line Project components would be underground in the Canton House's vicinity; no project features would be aboveground or visible. Project work would occur approximately 250 feet south of the Canton House's south elevation and south historic property boundary, and beneath East Lombard Street.

No physical impacts to the Canton House would occur; no project activity is proposed within the property's NRHP boundary. Therefore, no effects to the property's integrity of location, design, materials, and workmanship would occur.

The Canton House's integrity of setting is diminished due to the construction of non-historic multi-story buildings in its vicinity. All proposed project components would be located underground and would not alter the property's visual setting. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the property were identified. Therefore, project implementation would have no effect to the Canton House's integrity of setting.

Furthermore, no project activity would alter the property's feeling as an early twentieth-century, Colonial Revival-style building or its association with that style or with the Canton Company, the development of the Canton area, or Baltimore's manufacturing history. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the Canton House.

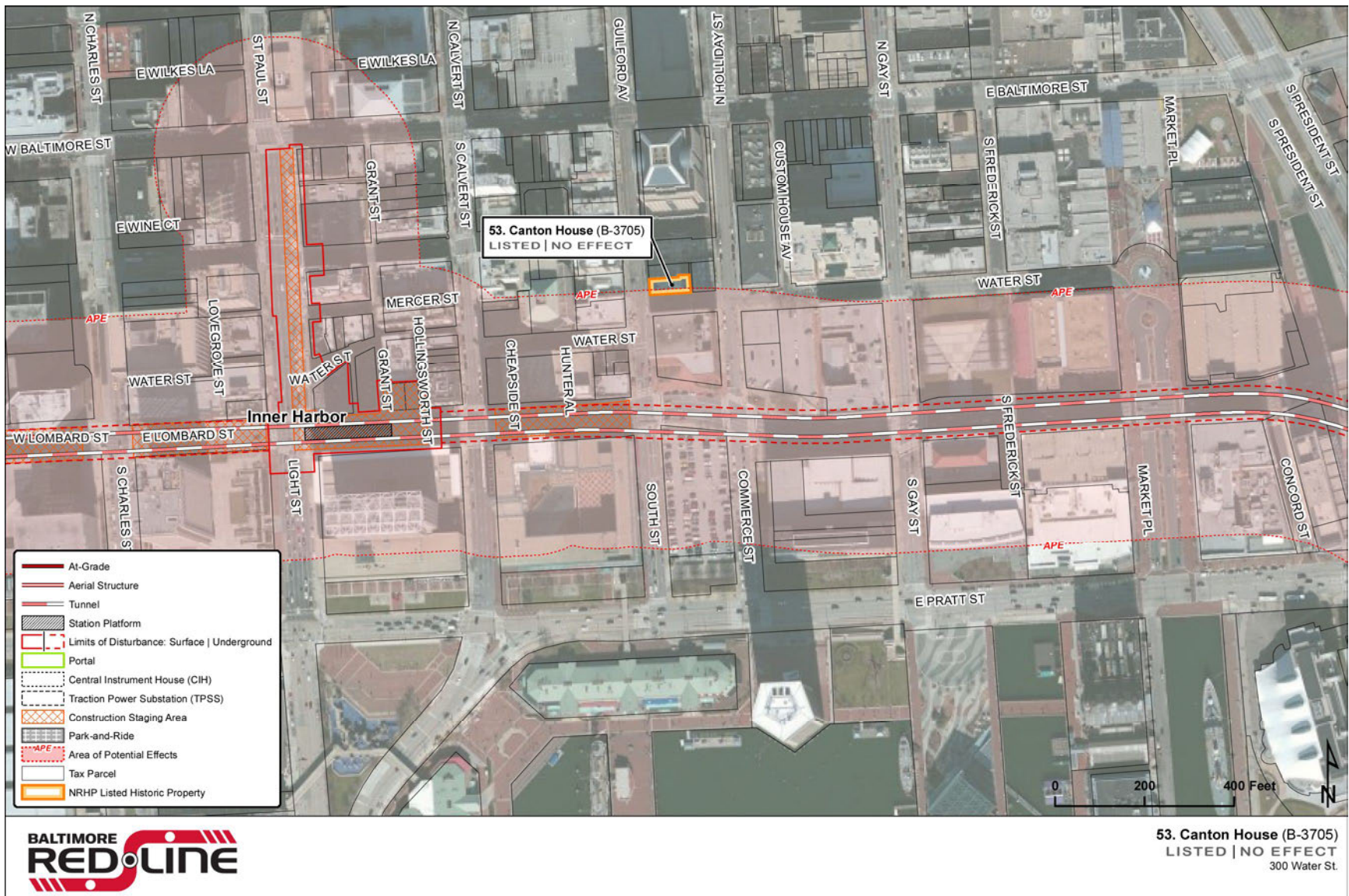


Figure 294. Proposed project in vicinity of Canton House



Figure 295. View northeast along South Street toward Canton House (see red arrow); proposed tunnel alignment in foreground beneath East Lombard Street



Figure 296. View southwest along South Street from Canton House toward the proposed tunnel alignment (beneath East Lombard Street)

54. Merchants' National Bank, site (Baltimore Federal Inner Harbor; USF&G)

301 Water Street

MIHP No. B-3687

The Merchants' National Bank was one of the few buildings to survive the Great Baltimore Fire of 1904. Originally constructed in 1894 as the Merchants' National Bank building, its carved granite enhances the facade's classical elements, including arched windows, embellished cornice lines, and Ionic pilasters. Following the Great Baltimore Fire, the first three stories were rebuilt. From 1929 to 1974 the building was occupied by the United States Fidelity and Guaranty Company, and since then by Baltimore Federal Savings and Loan. In the 1980s, the building was razed and replaced by an office building with the original facades preserved on Water, South, and Commerce streets. The building was listed in the NRHP under Criterion A for its association with developing commerce in Baltimore and its survival of the Great Baltimore Fire and under Criterion C for its classically inspired architecture. Merchants' National Bank is also within the NRHP-listed Business and Government Historic District.

Red Line Project components would be underground in the vicinity of the Merchants' National Bank. Underground project work would occur beneath East Lombard Street approximately 50 feet south of the property's south NRHP boundary. No proposed stations are proximate to the property.

Merchants' National Bank no longer retains integrity of design, materials, or workmanship. The historic property exists only as facades fronting Water, South, and Commerce streets, with the remaining building replaced by a non-historic parking garage and office building. Furthermore, no physical impacts to the property would occur; no project activity is proposed within the property's NRHP boundary. Therefore, no effects to the property's integrity of location, design, materials, and workmanship would occur.

Project implementation would not affect the Merchants' National Bank's setting as all project components would be located underground and would not be visible. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the property were identified. Therefore, project implementation would have no effect to the Merchants' National Bank's integrity of setting.

Because only the Merchants' National Bank's facade remains and the remaining portions of the building have been razed and replaced by a parking garage and office tower, the property no longer retains integrity of feeling or association. The bank no longer retains its feeling as a late-nineteenth- to early twentieth-century, classically styled bank building, or its association with Baltimore's developing commerce in the late-nineteenth century. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the Merchants' National Bank.

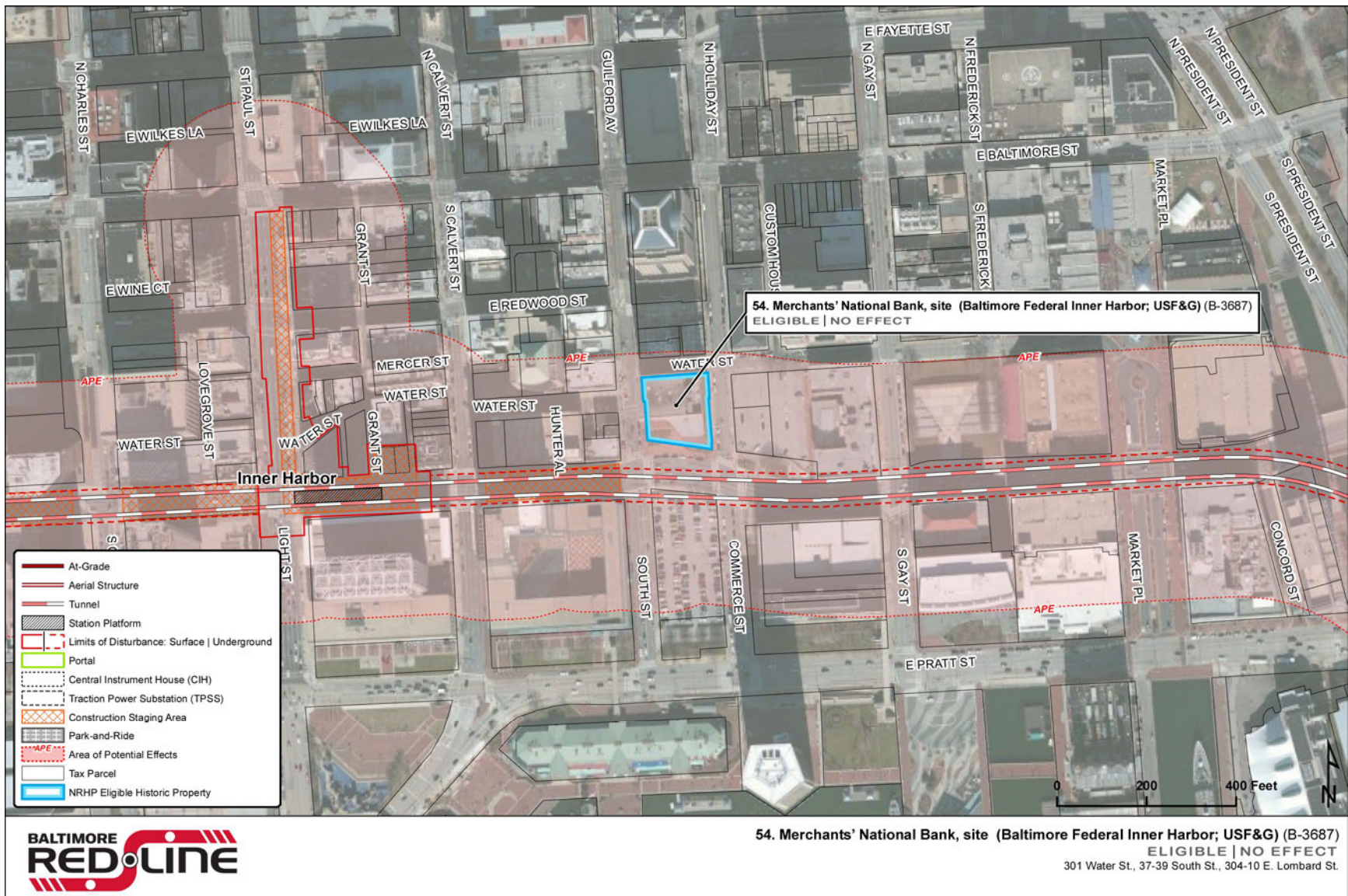


Figure 297. Proposed project in vicinity of Merchants' National Bank, site (Baltimore Federal Inner Harbor; USF&G)



Figure 298. View northeast along South Street toward the Merchants' National Bank, site (see red arrow), with proposed tunnel alignment in foreground beneath East Lombard Street



Figure 299. View southwest along South Street toward the proposed tunnel alignment (beneath East Lombard Street), with the Merchants' National Bank, site in south foreground



Figure 300. View northwest toward Commerce Street and the Merchants' National Bank, site (see red arrow); proposed tunnel alignment in foreground beneath East Lombard Street



Figure 301. View south from the Merchants' National Bank, site (in north foreground) along Commerce Street toward the proposed tunnel alignment beneath East Lombard Street

55. United States Custom House

40 South Gay Street

MIHP No. B-36

The United States Custom House is a six-story, U-shaped, granite-clad, Beaux Arts-style customs house building constructed from 1903 through late 1907. The building is distinguished by a low base course and rusticated basement carrying a three-story engaged colonnade, which in turn supports a full entablature and balustrade concealing an attic story and low roof. The ceiling of the Call Room, located in the pavilion, was painted by artist Francis Davis Millet and is acknowledged to be his masterpiece. The United States Customs House was listed in the NRHP under Criterion A for its association with the brief turn-of-the-twentieth-century movement to blend art and architecture in America, and under Criterion C as an exceptional example of a Beaux Arts-style building designed by the Washington D.C., firm Hornblower and Marshall. It is also located in the NRHP-listed Business and Government Historic District.

All Red Line Project components would be underground in the United States Custom House's vicinity; no project features would be aboveground or visible. Project work would occur along the United States Custom House's south historic property boundary, and beneath East Lombard Street.

At this time, no physical impacts to the United States Custom House would occur; no project activity is proposed within the property's NRHP boundary. If underpinning of the building would be required, the effect of that work would be assessed in the future under the terms of the Red Line Project's Programmatic Agreement. Therefore, no effects to the property's integrity of location, design, materials, and workmanship would occur.

The United States Custom House's integrity of setting is currently diminished due to the construction of non-historic buildings, surface parking lots, and the Holocaust Memorial Park in its vicinity and viewshed; however, the intact NRHP-listed Appraisers' Stores Building southeast of the property continues to contribute to the property's setting. All proposed project components would be located underground and would not alter the property's visual setting. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the property were identified. Therefore, project implementation would have no effect to the United States Custom House's integrity of setting.

Furthermore, no project activity would alter the property's feeling as an early twentieth-century, Beaux Arts-style customs house building, or its association with that style and the turn-of-the-twentieth-century movement to blend art and architecture in America. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the United States Custom House.

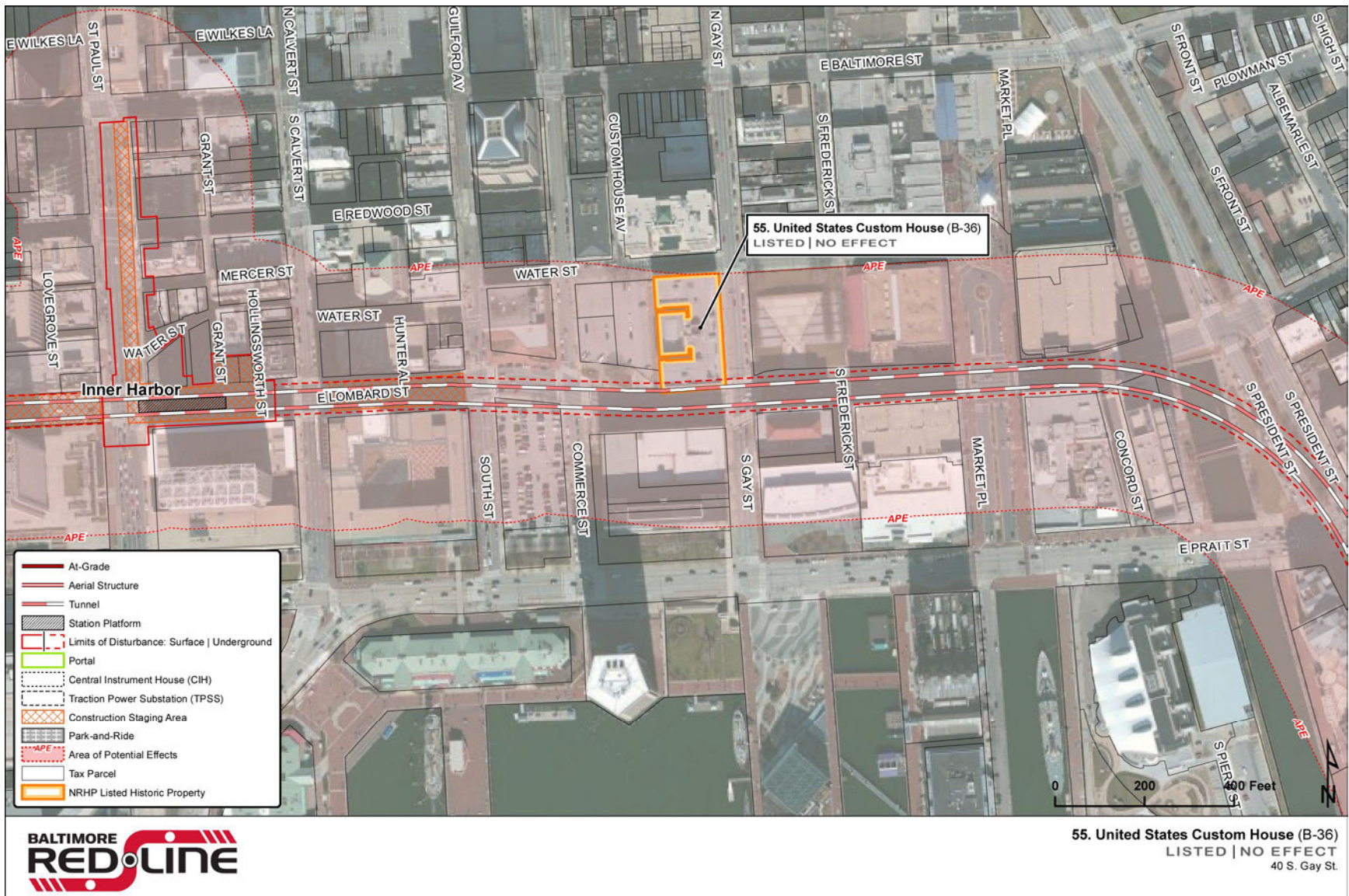


Figure 302. Proposed project in vicinity of United States Custom House



Figure 303. View east along proposed tunnel alignment (beneath East Lombard Street); United States Custom House to north



Figure 304. View northwest along proposed tunnel alignment (beneath East Lombard Street); United States Custom House to north



Figure 305. View south from the United States Custom House toward the proposed tunnel alignment beneath East Lombard Street



Figure 306. View southeast from the United States Custom House toward the proposed tunnel alignment beneath East Lombard Street

56. United States Appraisers' Stores (Appraisers' Stores Building)

103 South Gay Street

MIHP No. B-4496

The United States Appraisers' Stores (Appraisers' Stores Building) is a massive eight-story, rectangular, Art Deco-style building constructed of reinforced concrete clad in limestone at the first and second stories and brick on the remaining six stories. Constructed in 1932-1934, the building is distinguished by stylized limestone eagle statues at the parapet corners and a cast aluminum eagle statue surmounting the granite-framed main entrance facing South Gay Street. The Appraisers' Stores Building was determined eligible for listing in the NRHP under Criterion A for its association with the nearby United States Customs House and the continuing maritime activity that made Baltimore one of America's great ports, and under Criterion C as an excellent example of the Art Deco style designed by prominent Baltimore architects Taylor and Fisher in association with William F. Stone. It is also located in the NRHP-listed Business and Government Historic District.

All Red Line Project components would be underground in the Appraisers' Stores Building's vicinity; no project features would be aboveground or visible. Project work would occur just north of the Appraisers' Stores Building's north elevation and north historic property boundary, and beneath East Lombard Street.

No physical impacts to the Appraisers' Stores Building would occur; no project activity is proposed within the property's NRHP boundary. Therefore, no effects to the property's integrity of location, design, materials, and workmanship would occur.

The Appraisers' Stores Building's integrity of setting is diminished due to the construction of non-historic buildings and the Holocaust Memorial Park in its vicinity and viewshed; however, the intact NRHP-listed United States Custom House northwest of the property continues to contribute to the property's setting. All proposed project components would be located underground and would not alter the property's visual setting. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the property were identified. Therefore, project implementation would have no effect to the Appraisers' Stores Building's integrity of setting.

Furthermore, no project activity would alter the property's feeling as a 1930s Art Deco-style building, or its association with that style, the United States Customs House, or Baltimore's maritime history. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the United States Appraisers' Stores (Appraisers' Stores Building).

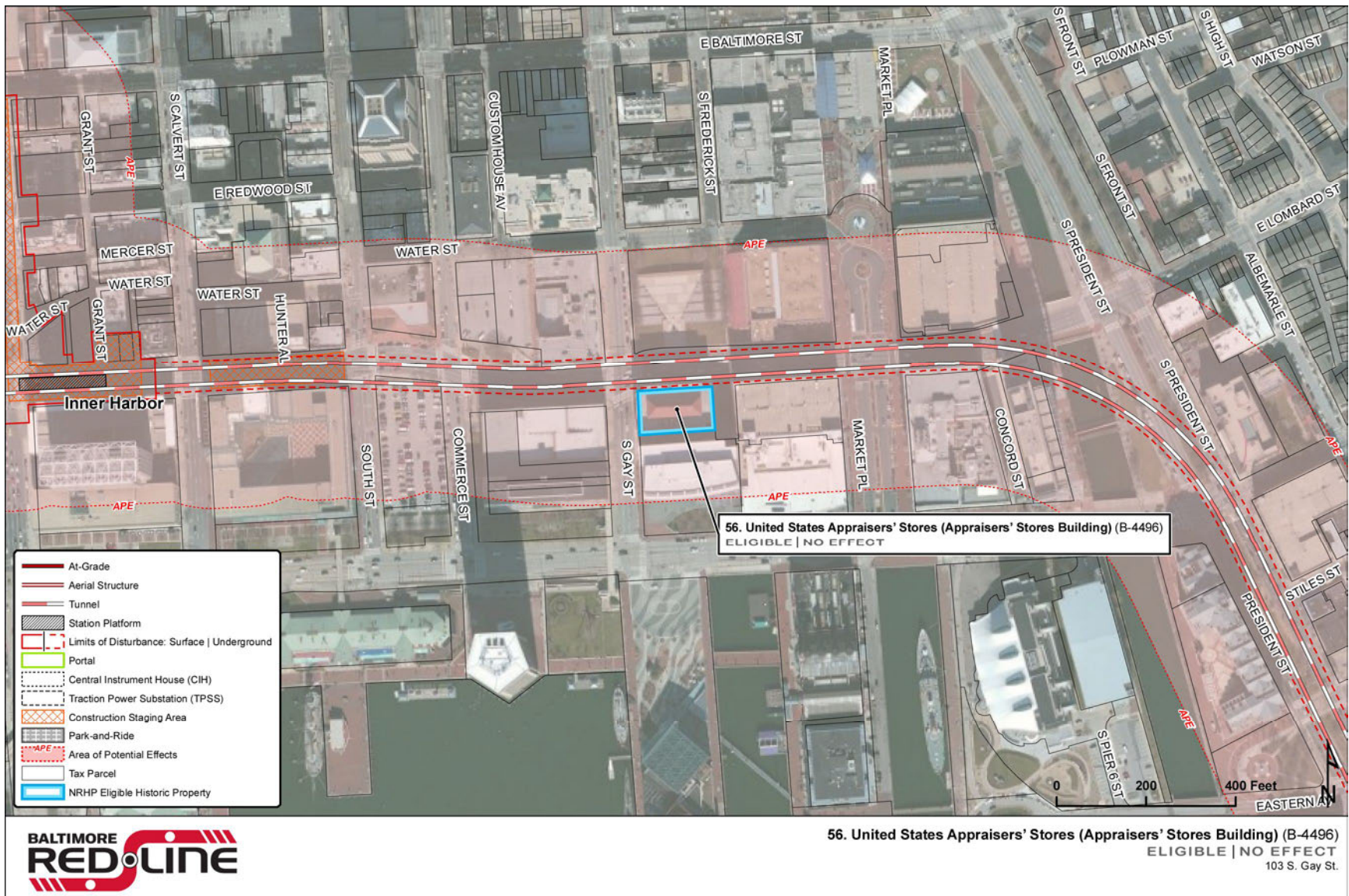


Figure 307. Proposed project in vicinity of United States Appraisers' Stores (Appraisers' Stores Building)



Figure 308. View southeast from South Frederick Street toward the proposed tunnel alignment (beneath East Lombard Street) and to the United State Appraisers' Stores



Figure 309. View northwest from the United States Appraisers' Stores toward the proposed tunnel alignment beneath East Lombard Street



Figure 310. View northeast from the United States Appraisers' Stores toward the proposed tunnel alignment beneath East Lombard Street

57. Candler Building (The Coca-Cola Building)

700 East Pratt Street

MIHP No. B-1002

The Candler Building (The Coca-Cola Building) is a twelve-story, brick-clad concrete industrial building with an eight-story rear portion at the building's southeast corner; a unifying cornice at the second story unites the two similar building sections. The building elevations are distinguished by evenly spaced window openings divided by engaged brick columns and stone courses between each floor. The building was constructed in 1912 by Asa G. Candler, Atlanta businessman and inventor of Coca-Cola, and one the earliest Baltimore examples of an industrial building constructed to offer small manufacturers office and work space. These industrial buildings provided ready-made spaces for a number of manufacturing operations and offices in a large, solid, fully electrified and modernized structure where the firms paid a set rent for the space and the building owners were responsible for utilities, security, and some maintenance. The Candler Building (The Coca Cola Building) was determined eligible for listing in the NRHP under Criterion C as an outstanding example of an early type of office building, which retained the character of nineteenth-century factory buildings, but on a large scale and with the ideal of efficiency which inspired modern industrial structures.

All Red Line Project components would be underground in the Candler Building's vicinity; no project features would be aboveground or visible. Project work would occur just north of the Candler Building's north elevation and north historic property boundary, and beneath East Lombard Street.

No physical impacts to the Candler Building would occur; no project activity is proposed within the property's NRHP boundary. Therefore, no effects to the property's integrity of location, design, materials, and workmanship would occur.

The Candler Building's integrity of setting is diminished due to the construction of nearby non-historic buildings, although some older buildings remain intact in its vicinity. All proposed project components would be located underground and would not alter the property's visual setting. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the property were identified. Therefore, project implementation would have no effect to the Candler Building's integrity of setting.

Furthermore, no project activity would alter the property's feeling as an early twentieth-century industrial office building or its association with that building type. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the Candler Building (The Coca-Cola Building).

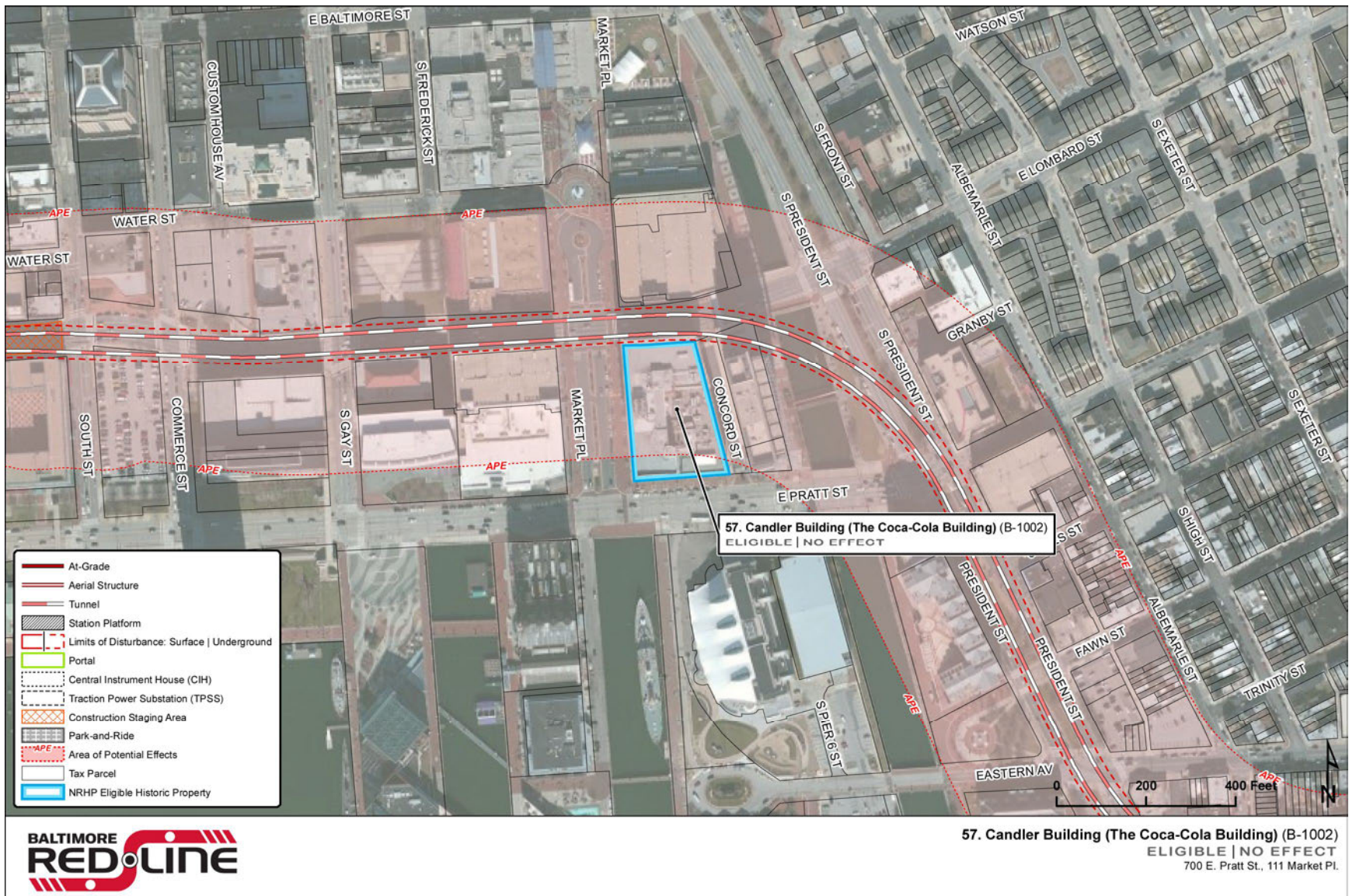


Figure 311. Proposed project in vicinity of Candler Building (The Coca-Cola Building)



Figure 312. View west along proposed tunnel alignment (beneath East Lombard Street); Candler Building on south



Figure 313. View northwest from the Candler Building along proposed tunnel alignment beneath East Lombard Street



Figure 314. View northeast from the Candler Building toward the proposed tunnel alignment beneath East Lombard Street

58. Jonestown Historic District

Irregularly shaped district roughly bounded by Jones Falls stream on the east; East Lexington Street on the north; Pratt Street on the south; and South Central Avenue on the east
MIHP No. B-2784

The Jonestown Historic District comprises a mix of industrial, commercial, and residential buildings on ten acres divided into twenty lots on the east side of the Jones Falls; the area was developed beginning in 1732 and continued into the twentieth century. The district contains many religious buildings, churches, and synagogues which were constructed by arriving immigrants; notable examples include the Greek Revival-style Lloyd Street Synagogue and the Victorian Gothic-style Russian Synagogue. Residential buildings consist of row houses and townhouses. The district also includes eight Baltimore City Landmarks, The Flag House National Historic Landmark, and the Shot Tower National Historic Landmark. The Jonestown Historic District was determined eligible for listing in the NRHP under Criterion A for its association with the coexisting residential, institutional, commercial, and industrial development of immigrant working class neighborhoods in Baltimore that supported the city's diversified manufacturing economy over three centuries. It was also determined eligible under Criterion C as a neighborhood of major religious, industrial, and institutional landmarks and residential and commercial row house structures that exemplify the small scale co-existence of industrial and residential land uses that spurred Baltimore's growth between the late eighteenth, nineteenth, and twentieth centuries.

All Red Line Project components would be underground in the Jonestown Historic District's vicinity; no project features would be aboveground or visible. Project work would occur underground beneath East Lombard and South President streets. The alignment would be constructed approximately 365 feet west of the Jonestown Historic District's west historic district boundaries and beneath East Lombard Street. From East Lombard Street, the alignment would shift south and continue beneath South President Street, approximately 165 feet west of the west historic district boundaries; between East Pratt and East Lombard streets, the historic district is separated from South President Street by a row of buildings along and oriented to that street.

No physical impacts to the Jonestown Historic District would occur; no project activity is proposed within the district's NRHP boundary. Therefore, no effects to the district's integrity of location, design, materials, and workmanship would occur.

Project implementation would not affect the Jonestown Historic District's integrity of setting as all proposed project components would be located underground and would not alter the district's visual setting. No historically significant views to or from the district would be obscured by project implementation, and no character-defining features of the district's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the district were identified. Therefore, project implementation would have no effect to the Jonestown Historic District's integrity of setting.

Furthermore, no project activity would alter the district's feeling as a collection of diverse religious, residential, commercial, industrial, and institutional architecture illustrating the

district's development over three centuries, or its association with represented building types, architectural styles, or Baltimore's manufacturing economy and culture. Therefore, project implementation would have no effect to the district's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the Jonestown Historic District.

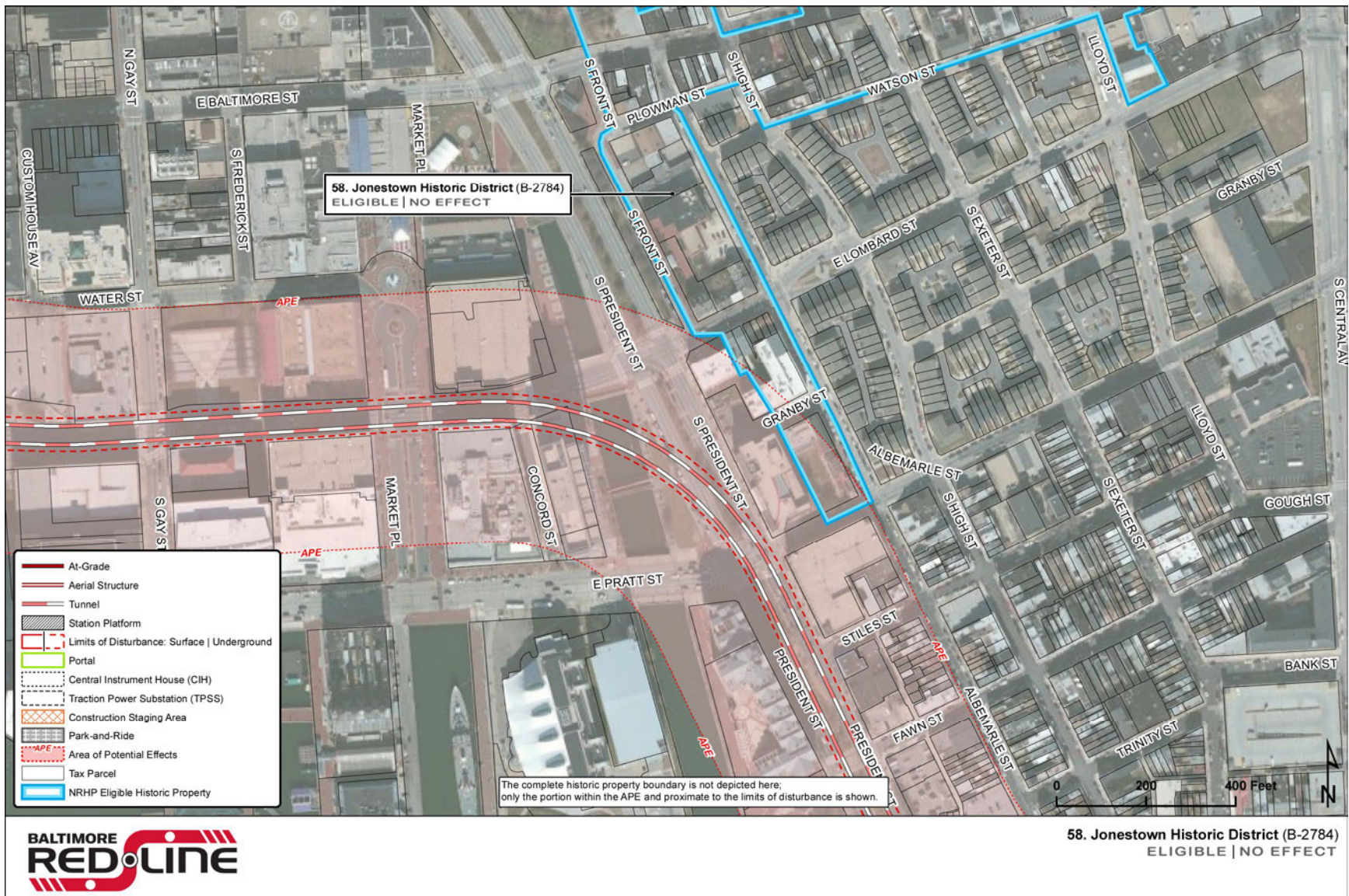


Figure 315. Proposed project in vicinity of Jonestown Historic District



Figure 316. View northeast toward the Jonestown Historic District from proposed tunnel alignment beneath East Lombard Street at its transition to beneath South President Street



Figure 317. View southwest within Jonestown Historic District (at East Lombard and South Albemarle streets) to proposed alignment transition beneath East Lombard Street to beneath South President Street



Figure 318. View east from the proposed tunnel alignment (beneath South President Street) toward the southern end of the Jonestown Historic District at East Pratt Street



Figure 319. View west toward the proposed tunnel alignment (beneath South President Street) from the southern end of the Jonestown Historic District at East Pratt Street

59. Star-Spangled Banner Flag House

844 East Pratt Street

MIHP No. B-15

The Star-Spangled Banner Flag House is a two-and-a-half-story, salmon brick-clad, Federal-style corner row house distinguished by two brick chimneys and gabled dormers on the hipped roof. The building was constructed circa 1793 and expanded in 1820. The City of Baltimore purchased the building in 1927 and later restored it to its original appearance, with the exception of a one-and-a-half-story museum building that was constructed in 1953 at the rear of the building and connected to the main house by a covered breezeway. The Star-Spangled Banner Flag House was listed in the NRHP under Criterion A as the home of Mary Young Pickersgill, who sewed the Star Spangled Banner garrison flag that flew over Fort McHenry in the summer of 1814 during the Battle of Baltimore in the War of 1812 and inspired Francis Scott Key to write a poem that eventually became the national anthem. The building is also a National Historic Landmark and located within the NRHP-eligible Jonestown Historic District.

All Red Line Project components would be underground in the Star-Spangled Banner Flag House's vicinity; no project features would be aboveground or visible. Project work would occur approximately 140 feet west of the Star Spangled Banner Flag House's west elevation and west historic property boundary, and beneath South President Street.

No physical impacts to the Star-Spangled Banner Flag House would occur; no project activity is proposed within the property's NRHP boundary. Therefore, no effects to the property's integrity of location, design, materials, and workmanship would occur.

The Star-Spangled Banner Flag House does not retain integrity of setting due to the construction of non-historic buildings in its vicinity. All proposed project components would be located underground and would not be visible. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the property were identified. Therefore, project implementation would have no effect to the Star-Spangled Banner Flag House's integrity of setting.

Furthermore, no project activity would alter the property's feeling as a late-eighteenth-century, Federal-style row house, or its association with Mary Young Pickersgill, the Star-Spangled Banner garrison flag, the Battle of Baltimore, or the national anthem written by Francis Scott Key. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the Star-Spangled Banner Flag House.

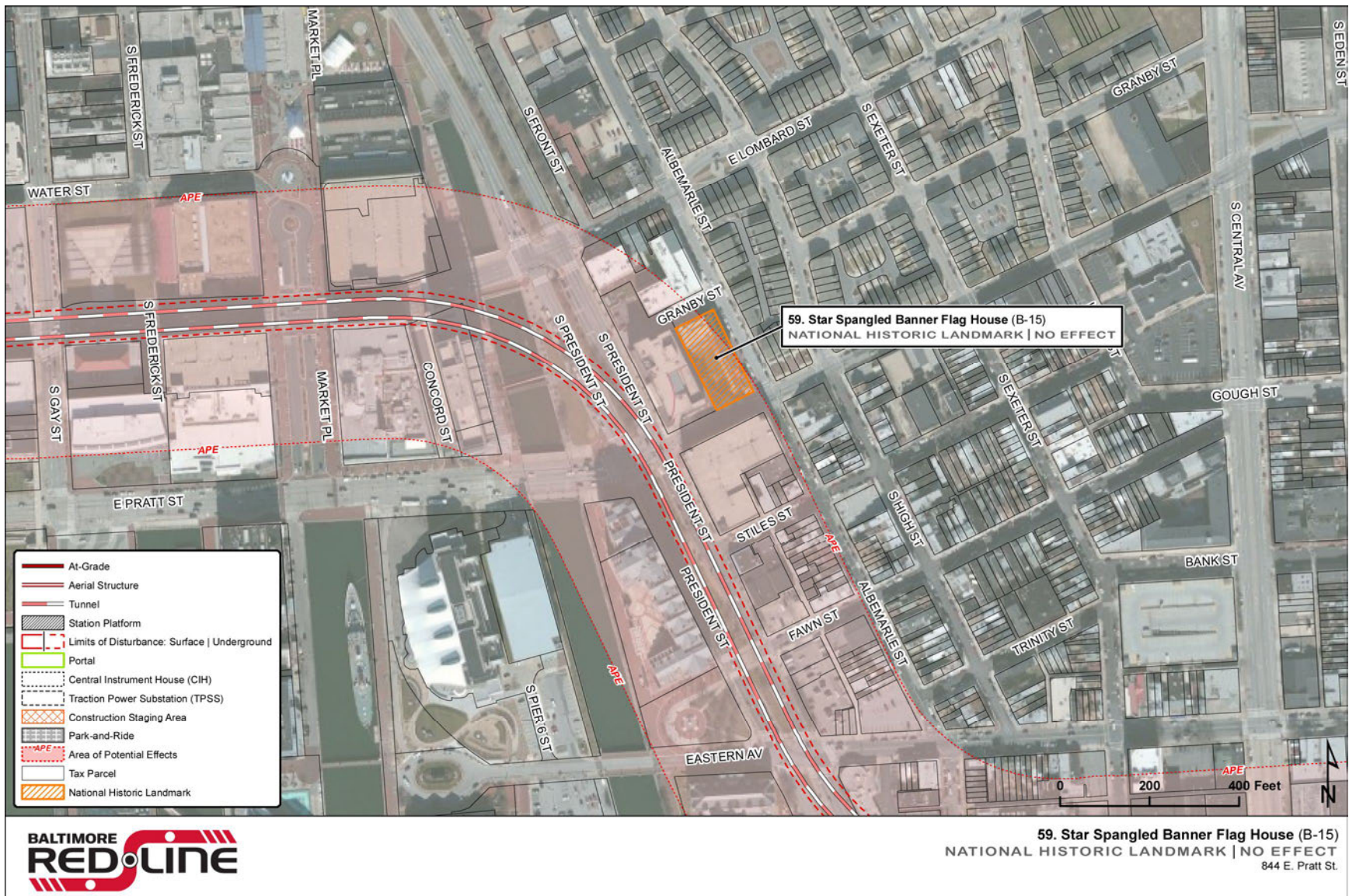


Figure 320. Proposed project in vicinity of Star Spangled Banner Flag House



Figure 321. View east along East Pratt Street from the proposed tunnel alignment (beneath South President Street) toward the Star Spangled Banner Flag House



Figure 322. View west along East Pratt Street toward the proposed tunnel alignment (beneath South President Street) from the Star Spangled Banner Flag House

60. Little Italy Historic District

Roughly bounded by Eastern Avenue, President Street, Pratt Street, and Central Avenue
MIHP No. B-5121

The Little Italy Historic District is a compact residential neighborhood with mostly small two- and three-story brick row houses, many of which have been re-sided with Formstone. The row houses are interspersed by commercial properties and a small number of institutional properties, such as St. Leo's Roman Catholic Church. The circa 1830-1930 district is largely characterized by buildings directly fronting on sidewalks lining narrow, short streets. Italian immigrants began settling in the Little Italy Historic District in the mid-nineteenth century as they took jobs along the nearby waterfront or with the railroad. The Italian population continued growing throughout the late nineteenth and early twentieth centuries and was almost exclusively Italian by the 1920s. A distinct neighborhood culture was established, characterized by multi-generational Italian-owned businesses and residents who stayed in the neighborhood their entire lives, passing their houses on to the next generation. The district was determined eligible for listing in the NRHP under Criterion A for its association with immigrant and settlement patterns in Baltimore and its embodiment as an Italian-American ethnic neighborhood, and under Criterion C as a typical Baltimore rowhouse neighborhood.

All Red Line Project components would be underground in the Little Italy Historic District's vicinity; no project features would be aboveground or visible. Project work would occur approximately 90 to 120 feet west of the district's westernmost NRHP boundary and contributing buildings, and 50 feet southwest of the district's southwest corner, beneath South President Street.

No physical impacts to the Little Italy Historic District would occur; no project activity is proposed within the district's NRHP boundary. Therefore, no effects to the district's integrity of location, design, materials, and workmanship would occur.

Project implementation would not affect the Little Italy Historic District's integrity of setting as all project components would be located underground and would not be visible. No historically significant views to or from the district would be obscured by project implementation and no character-defining features of the district's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the district were identified. Therefore, project implementation would have no effect to the Little Italy Historic District's integrity of setting.

Furthermore, no project activity would alter the district's feeling as an Italian-American ethnic neighborhood of residential, commercial, and institutional buildings, or its association with immigrant and settlement patterns in Baltimore. Therefore, project implementation would have no effect to the district's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the Little Italy Historic District.

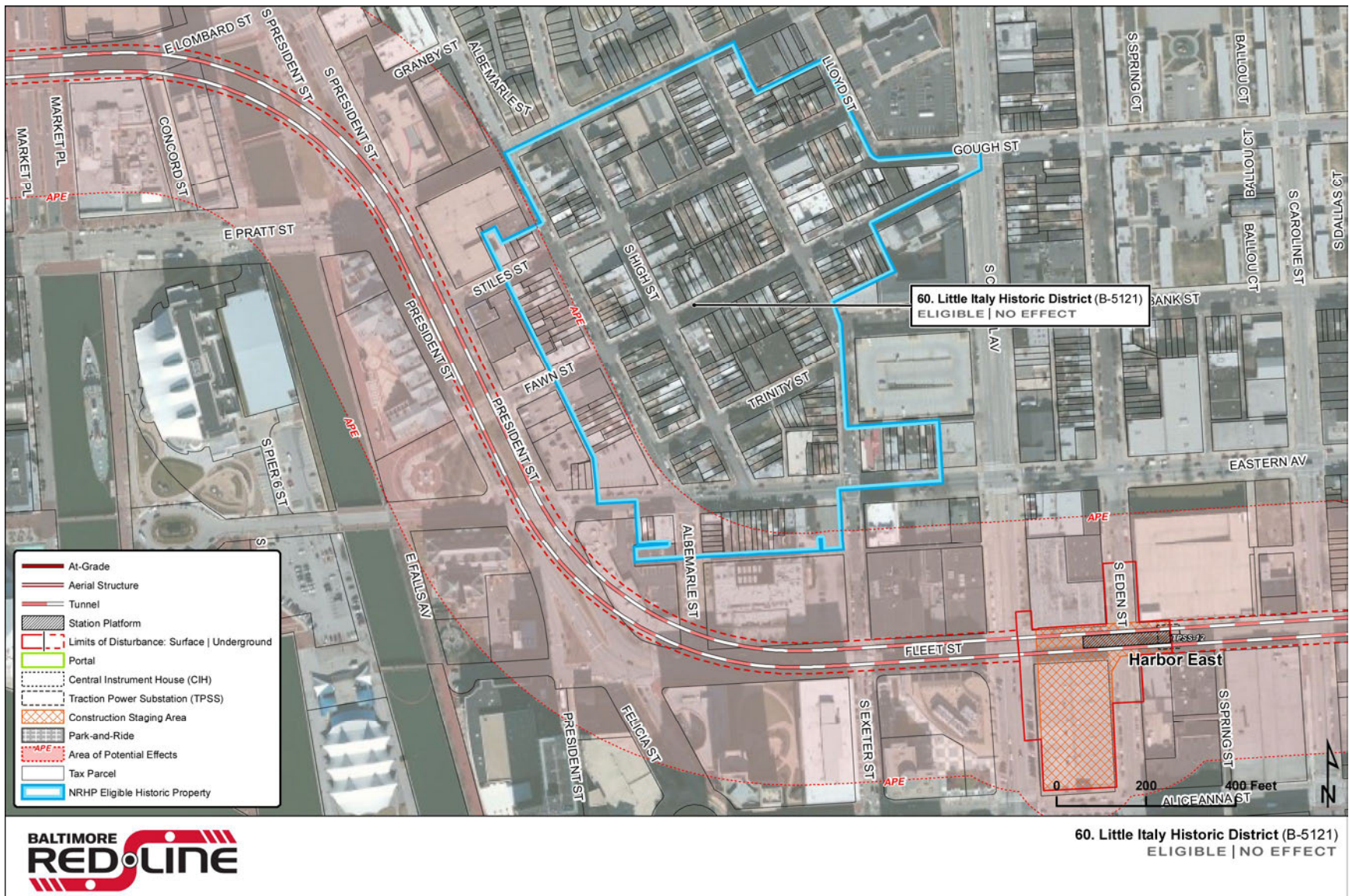


Figure 323. Proposed project in vicinity of Little Italy Historic District



Figure 324. View east from the proposed tunnel alignment (beneath South President Street) toward the Little Italy Historic District at Stiles Street

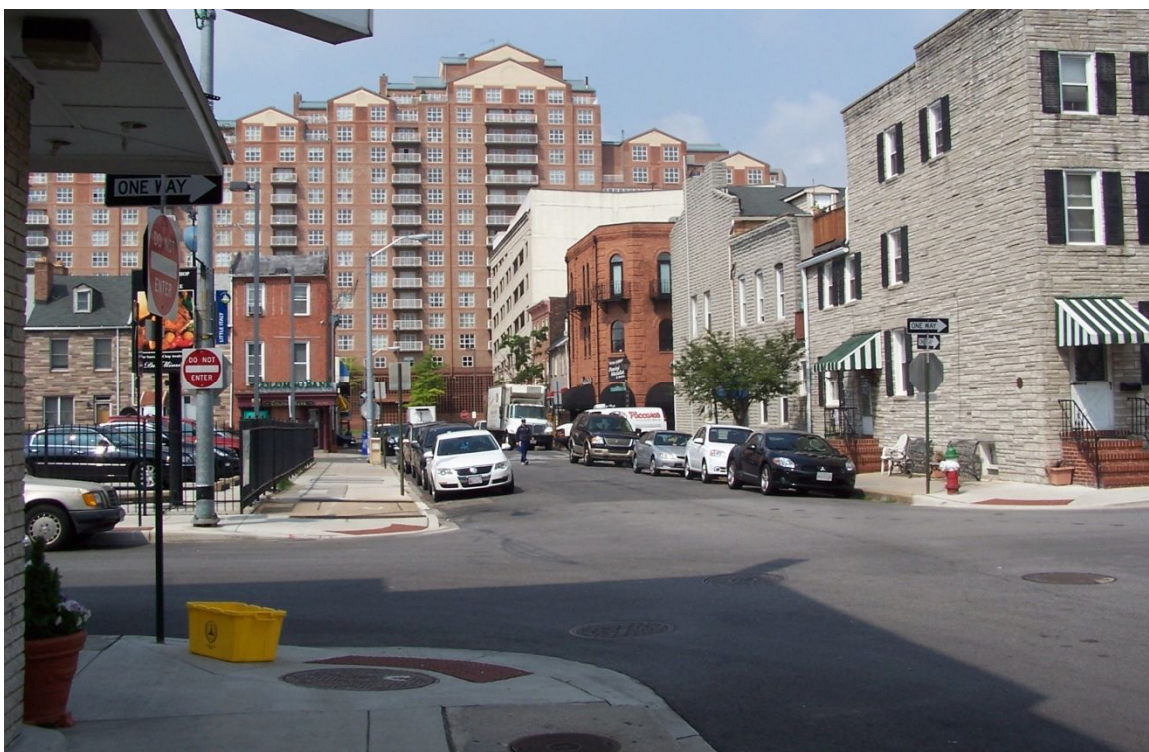


Figure 325. View southwest toward the proposed tunnel alignment (beneath South President Street) from within the Little Italy Historic District at Stiles and High streets



Figure 326. View west toward the proposed tunnel alignment beneath South President Street from the Little Italy Historic District's southwest boundary at Eastern Avenue and Albemarle Street



Figure 327. View south from within the Little Italy Historic District at Eastern Avenue and Albemarle Street toward the proposed tunnel alignment beneath Fleet Street at its transition from beneath South President Street

61. Eastern Avenue Pumping Station

751 Eastern Avenue

MIHP No. B-1047

The Eastern Avenue Pumping Station is a monumental Classical Revival-style, three-and-a-half-story municipal building faced with orange brick and trimmed with sandstone and granite. It was constructed in 1910-11 by the City of Baltimore to improve their sewage system by pumping sewage to a treatment plant in Baltimore County rather than the cesspools and open drains used previously. It was determined eligible for the NRHP under Criterion A for its association with the development of Baltimore's public sewerage system that aided in the expansion of city and municipal services after 1912, and under Criterion C as an excellent example of a Classical Revival-style municipal facility designed by architect Henry Brauns.

All Red Line Project components would be underground in the Eastern Avenue Pumping Station's vicinity; no project features would be aboveground or visible. Project work would occur approximately 35 feet east of the Eastern Avenue Pumping Station's historic property boundary and beneath South President Street.

No physical impacts to the Eastern Avenue Pumping Station would occur; no project activity is proposed within the property's NRHP boundary. Therefore, no effects to the property's integrity of location, design, materials, and workmanship would occur.

The Eastern Avenue Pumping Station no longer retains integrity of setting due to the construction of non-historic buildings in its vicinity and viewshed and a large parking lot to the property's northeast. All proposed project components would be located underground and would not alter the property's visual setting. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the property were identified. Therefore, project implementation would have no effect to the Eastern Avenue Pumping Station's integrity of setting.

Furthermore, no project activity would alter the property's feeling as an early twentieth-century Classical Revival-style municipal building, or its association with that style or with the development of Baltimore's public sewerage system. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the Eastern Avenue Pumping Station.

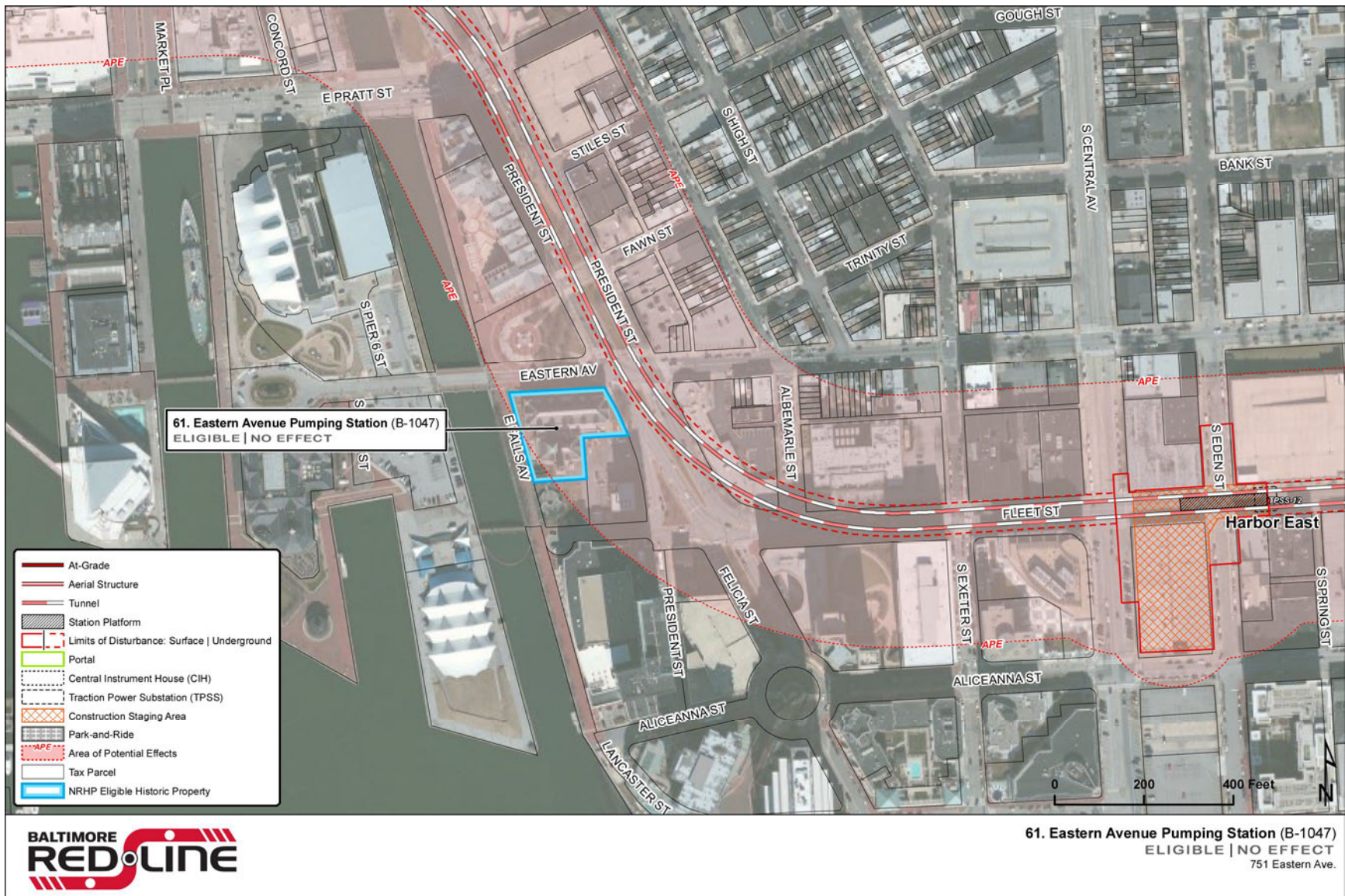


Figure 328. Proposed project in vicinity of Eastern Avenue Pumping Station



Figure 329. View south toward the Eastern Avenue Pumping Station from the proposed tunnel alignment beneath South President Street



Figure 330. View northeast from the Eastern Avenue Pumping Station toward the proposed tunnel alignment beneath South President Street



Figure 331. View southeast from the Eastern Avenue Pumping Station toward the proposed tunnel alignment at its transition from beneath South President Street to beneath Fleet Street

62. President Street Station (Philadelphia, Wilmington & Baltimore Railroad Station)

President and Fleet streets

MIHP No. B-3741

The President Street Station is a two-story, brick, Italianate-style headhouse distinguished by a barrel-vault roof. The surviving headhouse portion was constructed in 1849-1850 by the Philadelphia, Wilmington & Baltimore Railroad; a trainshed originally extended from the building's east elevation and has since been demolished. The headhouse contained passenger ticketing and waiting facilities and the offices of the railroad company. The building's original facade and entrance was located on its west elevation and has since been relocated to its east elevation fronting South President Street. It was listed in the NRHP under Criterion A as the second oldest train station remaining in the City of Baltimore, for its association with the development of the city's railroad network, and for its association with events in the Civil War in Maryland.

All Red Line Project components would be underground in the President Street Station's vicinity; no project features would be aboveground or visible. Project work would occur approximately 150 feet northeast of the President Street Station's north elevation and along and near its north historic property boundary, beneath South President Street.

No physical impacts to the President Street Station would occur; no project activity is proposed within the property's NRHP boundary. In addition, the property's integrity of design, materials, and workmanship is diminished due to the loss of the trainshed and the subsequent incompatible materials used on the remaining headhouse's east elevation, as well as the relocation of the building entrance to this elevation. Therefore, no effects to the property's integrity of location, design, materials, and workmanship would occur.

The President Street Station no longer retains integrity of setting due to the construction of non-historic buildings in its vicinity and viewshed. No historically significant views to or from the property were identified or would be obscured. All proposed project components in the vicinity of the property would be located underground and would not further alter the property's visual setting. Because no historically significant views would be obscured, no visual effects to the property were identified. Therefore, project implementation would have no effect to the President Street Station's integrity of setting.

Furthermore, no project activity would alter the property's feeling as a mid-nineteenth-century, Italianate-style headhouse, or its association with that style, the development of Baltimore's railroad network, or Civil War events. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the President Street Station.

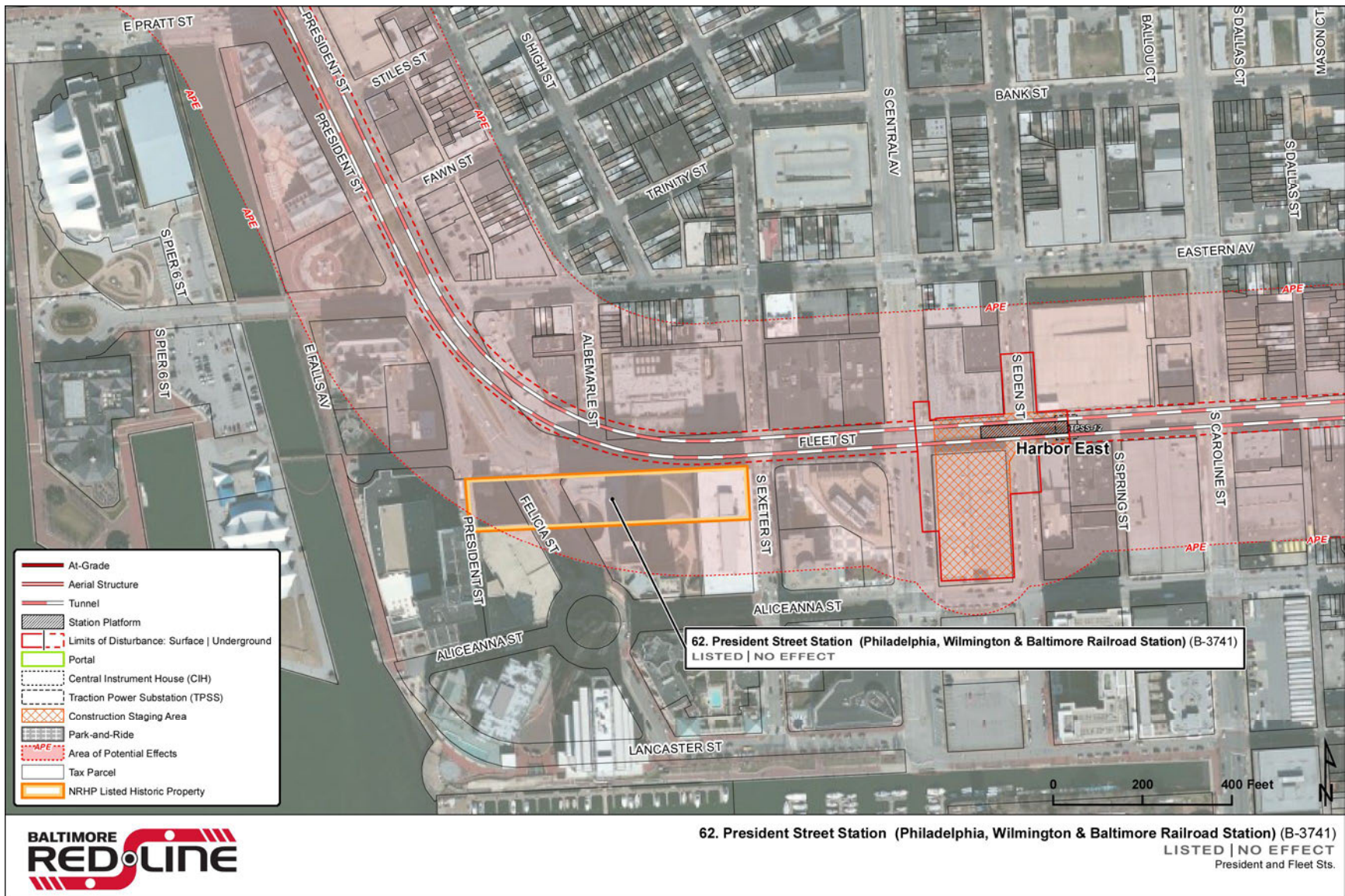


Figure 332. Proposed project in vicinity of President Street Station (Philadelphia, Wilmington & Baltimore Railroad Station)



Figure 333. View southwest toward the proposed tunnel alignment at its transition from beneath South President Street to beneath Fleet Street and to the President Street Station



Figure 334. View northeast from the President Street Station toward the proposed tunnel alignment at its transition from beneath South President Street to beneath Fleet Street



Figure 335. View east from the President Street Station toward the proposed tunnel alignment at its transition from beneath South President Street to beneath Fleet Street

63. South Central Avenue Historic District

See map for historic district boundaries

MIHP No. B-5058

The South Central Avenue Historic District comprises primarily low-scale residential and industrial buildings, exemplifying the mixed-use neighborhood in Baltimore. Evolving from the early-nineteenth through the mid-twentieth century, the district contains primarily brick buildings that demonstrate changes in use over time. Many ancillary industries that supported more prominent industrial enterprises were present within the historic district. Immigrant labor and access to transportation routes allowed the area to maintain its industrial and residential character. The South Central Avenue Historic District was listed in the NRHP in 2001. It is significant under Criterion A for its association with Baltimore's industrial history and for demonstrating the city's economic changes in this area for more than 150 years.

Red Line Project activity in the vicinity of the South Central Avenue Historic District would be extensive, but the majority of work would occur outside of or beneath the historic district's NRHP boundary. Only a small section of sidewalk on Eden Street north of Fleet Street would be within the project's limits of disturbance. The Harbor East Station, which would be below ground, would be located beneath the Fleet Street/Eden Street intersection. The station would be built using cut-and-cover construction, which would connect with bored sections of tunneling for the underground alignment. Underground station construction would occur outside and just south of the historic district within the street footprint on Fleet Street between South Central Avenue and Eden Street; construction would require up to four years and is not considered a temporary impact because of this time period's length. Construction staging would also occur on the block south of Fleet Street between South Central Avenue and Eden Street, outside of the historic district boundary. A fan plant/vent structure would also be located at this location; the precise materials used to clad this structure have not been determined, but it would be between four and six stories in height with a square footprint measuring 50feet by 50 feet.

The South Central Avenue Historic District retains integrity of location, design, materials, and workmanship. With the exception of a small corner of sidewalk, all project work would occur outside of or beneath the historic district boundaries. Therefore the Red Line Project would have no adverse effect to the district's integrity of location, design, materials, and workmanship.

The South Central Avenue Historic District retains moderate integrity of setting. This setting occurs within and immediately outside of the district, which also abuts the Fell's Point Historic District. Changes, including new construction, outside of the district have occurred to its south and southwest; additional project changes in this area would result in a cumulative visual adverse effect. Construction impacts, which would cause substantial disruption on streets just outside of the district for an extended period, coupled with the proposed fan plant/vent structure, would contribute to this adverse effect to the South Central Avenue Historic District's setting by introducing visual elements out of character with the district's intact historic setting. Therefore, project implementation would have an adverse effect to the South Central Avenue Historic District's integrity of setting.

The South Central Avenue Historic District retains integrity of feeling and association. Character-defining features that convey the district's expression of its historic industrial significance at a distinct period in time, as well as its association with industrial growth in Baltimore, are present and would not be affected by the Red Line Project. Therefore the project would have no effect on the South Central Avenue Historic District's integrity of feeling and association.

Based on this evaluation, the Red Line Project would have an **adverse effect** to the South Central Avenue Historic District.

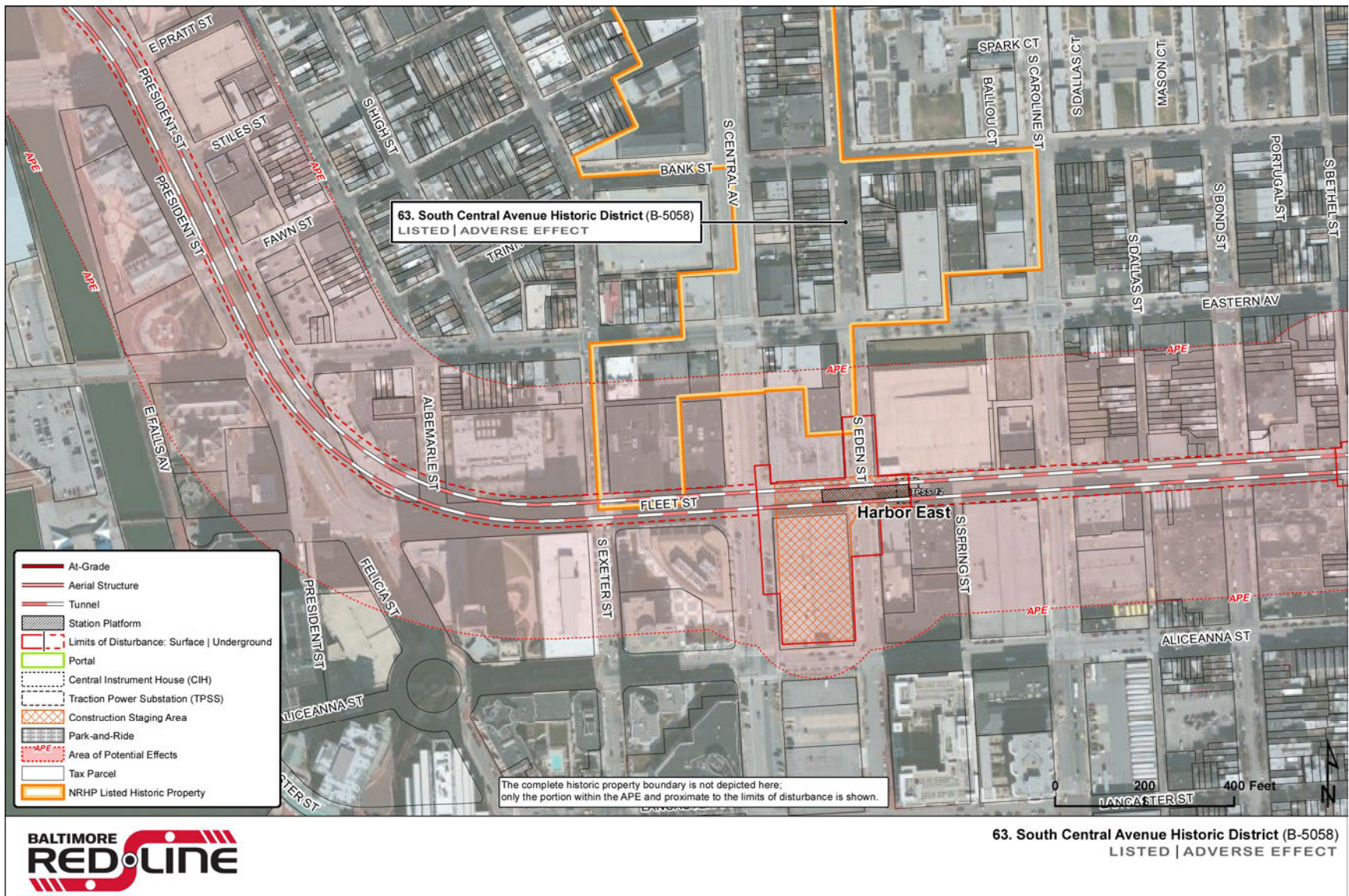


Figure 336. Proposed project in vicinity of the South Central Avenue Historic District



Figure 337. View southeast toward the South Central Avenue Historic District (from Eastern Avenue and South Exeter Street); proposed tunnel alignment to south beneath Fleet Street



Figure 338. View northwest toward the South Central Avenue Historic District (at South Central Avenue) from the proposed tunnel alignment beneath Fleet Street



Figure 339. View south along South Central Avenue toward the proposed tunnel alignment (beneath Fleet Street) from South Central Avenue Historic District's south boundary



Figure 340. View south from South Central Avenue Historic District's south boundary (at South Eden Street) toward the proposed tunnel alignment and Harbor East Station (both beneath Fleet Street)

64. Bagby Furniture Company Building

509-521 South Exeter Street

MIHP No. B-1011

The Bagby Furniture Company Building is a four-story, U-shaped, brick factory building comprised of three sections constructed between 1902 and 1907. The building is characterized by original brick architectural details and was modernized with new windows and a first floor office space in 1950. It was listed in the NRHP under Criterion A for its association with the evolution of Baltimore's furniture industry, and under Criterion B for its association with William Hugh Bagby, who was a major figure in the wholesale furniture business.

Red Line Project components in the vicinity of the Bagby Furniture Company Building would include the underground alignment along the building's south boundary, beneath Fleet Street, and the Harbor East Station, which would also be below ground and beneath the Fleet Street/Eden Street intersection. The station would be approximately 200 feet east of the property and built using cut-and-cover construction, which would connect with bored sections of tunneling for the underground alignment. Underground station construction would occur outside and just south of the historic district within the street footprint on Fleet Street between South Central Avenue and Eden Street; construction would require up to four years and is not considered a temporary impact because of this time period's length. A fan plant/vent structure would also be located at this location; the precise materials used to clad this structure have not been determined, but it would be between four and six stories in height with a square footprint measuring 50 feet by 50 feet.

No physical impacts to the Bagby Furniture Company Building would occur; no project activity is proposed within the property's NRHP boundary. Therefore, no effects to the property's integrity of location, design, materials, and workmanship would occur.

Project implementation would not affect the Bagby Furniture Company Building's integrity of setting. Project components would be located underground or to the building's east where the property does not retain integrity of setting due to the construction of non-historic buildings. No historically significant views to or from the property would be obscured by project implementation and no character-defining features of the property's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the property were identified. Therefore, project implementation would have no effect to the Bagby Furniture Company Building's integrity of setting.

Furthermore, no project activity would alter the property's feeling as a furniture factory building, or its association with the evolution of Baltimore's furniture industry or prominent businessman William Hugh Bagby. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the Bagby Furniture Company Building.

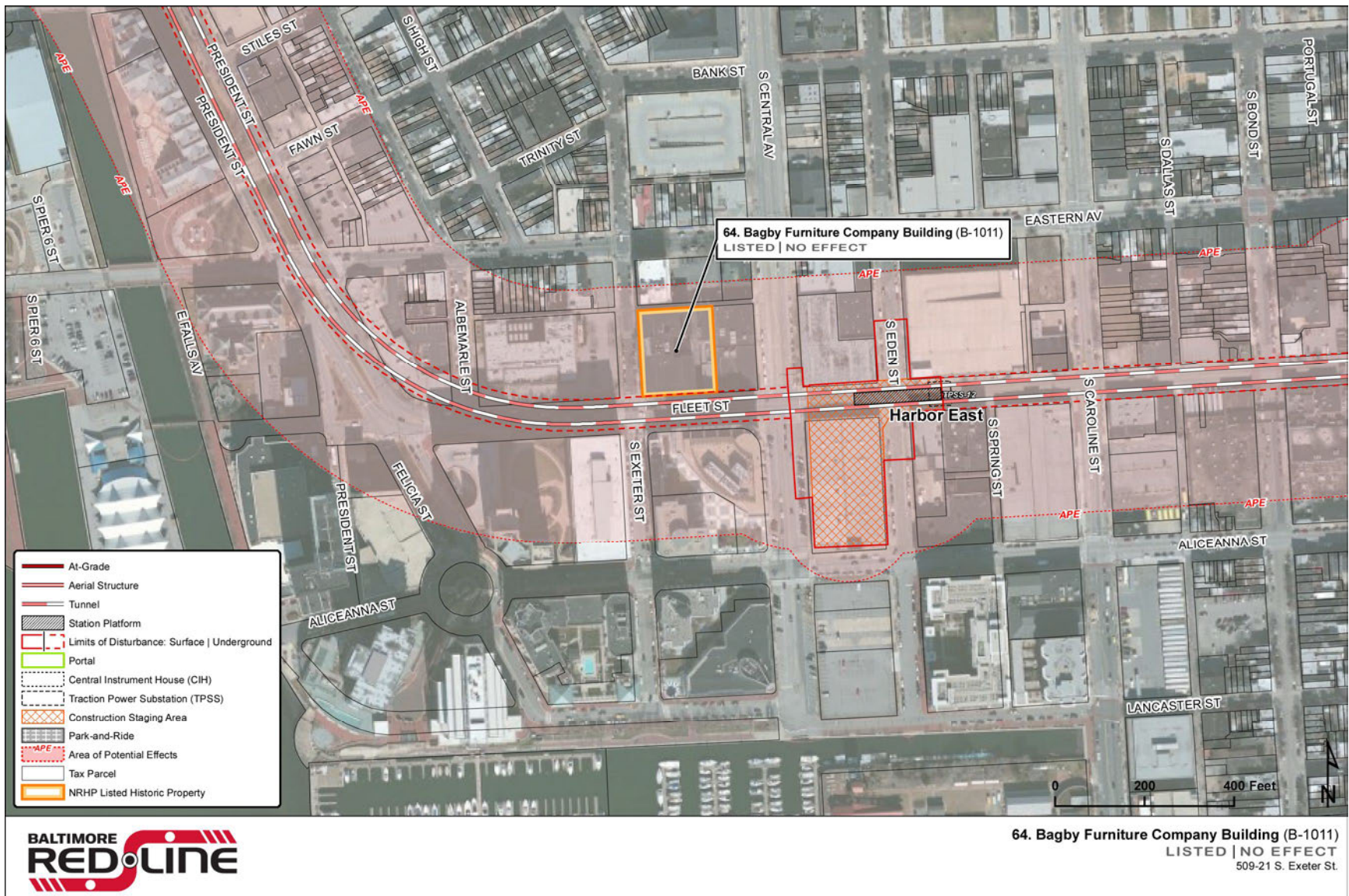


Figure 341. Proposed project in vicinity of Bagby Furniture Company Building



Figure 342. View southwest toward the proposed tunnel alignment (beneath Fleet Street) from the Bagby Furniture Company Building



Figure 343. View southeast from South Exeter Street toward the proposed tunnel alignment (beneath Fleet Street); Bagby Furniture Company Building in east foreground



Figure 344. View northeast at South Exeter Street toward the Bagby Furniture Company Building and along proposed tunnel alignment (beneath Fleet Street)

65. Harford Run Headwall and Drain

Under South Central Avenue from just north of Orleans Street to the Inner Harbor; see map for precise historic property boundaries

MIHP No. B-5054

The Harford Run Headwall and Drain is a brick-and-ashlar stone drain constructed on an existing canal between 1870 to the mid-twentieth century. Harford Run, a natural water feature, originally wound its way through Baltimore and drained into Baltimore Harbor. By the early 1820s, the City of Baltimore straightened Harford Run into a canal. The project was one of the earliest public works projects undertaken by the City of Baltimore and aimed to control storm water and sewage treatment in the central part of the city. The Harford Run Headwall and Drain was determined eligible for listing in the NRHP under Criterion A for its association with the early development of Baltimore City's waste water management and public works, and under Criterion C for its engineered design.

Red Line Project activity near the Harford Run Headwall and Drain would be extensive due to the presence of the Harbor East Station, which would be below ground. The historic Harford Run Headwall and Drain is also below ground in the areas proximate to the project. The project alignment, located beneath Fleet Street on an east-west axis, would also run beneath the Harford Run Headwall and Drain, which is on a north-south axis beneath South Central Avenue. However, no work would occur within the headwall/drain's historic boundary; all work would occur beneath it. The Harbor East Station would be built directly east of the tunnel. The station would be constructed using cut-and-cover tunneling techniques that would adjoin the project alignment's bored tunnel that would pass beneath the Harford Run Headwall and Drain. Station construction would require substantial disruption on Fleet Street and to the Harford Run Headwall and Drain's east. Station entrances and a fan plant/vent structure of four to six stories in height would be incorporated into the area. Construction activities are anticipated to last for approximately four years; because of this time span, they are not considered temporary impacts. However, because the Harford Run Headwall and Drain is below grade at this area, it would not be impacted. No potential vibration impacts have been identified during project studies; however, extensive monitoring of the drain would occur during construction activities to ensure that no adverse vibration impacts or settling would occur. Monitoring is being planned for both safety and historic impact reasons in response to the sensitive nature of this resource.

No physical impacts to the Harford Run Headwall and Drain would occur; no project activity is proposed within the property's NRHP boundary, although the project alignment would run beneath the tunnel. Therefore, no effects to the property's integrity of location, design, materials, or workmanship would occur.

The Harford Run Headwall and Drain retains integrity of setting. Only below-ground segments are within the APE. The project alignment extending beneath the tunnel would not affect its integrity of setting. Therefore, project implementation would have no effect on the Harford Run Headwall and Drain's integrity of setting, which is unique because it is primarily a subsurface built historic property.

The Harford Run Headwall and Drain retains integrity of feeling and association. Character-defining features that convey the drain's engineering technology and period in time, as well as its association with infrastructure in Baltimore, are present and would not be affected by the Red Line Project; within the project's APE, the drain is entirely underground. Therefore, the project would have no effect on the Harford Run Headwall and Drain's integrity of feeling and association.

Based on this evaluation, the Red Line Project would have **no effect** to the Harford Run Headwall and Drain.

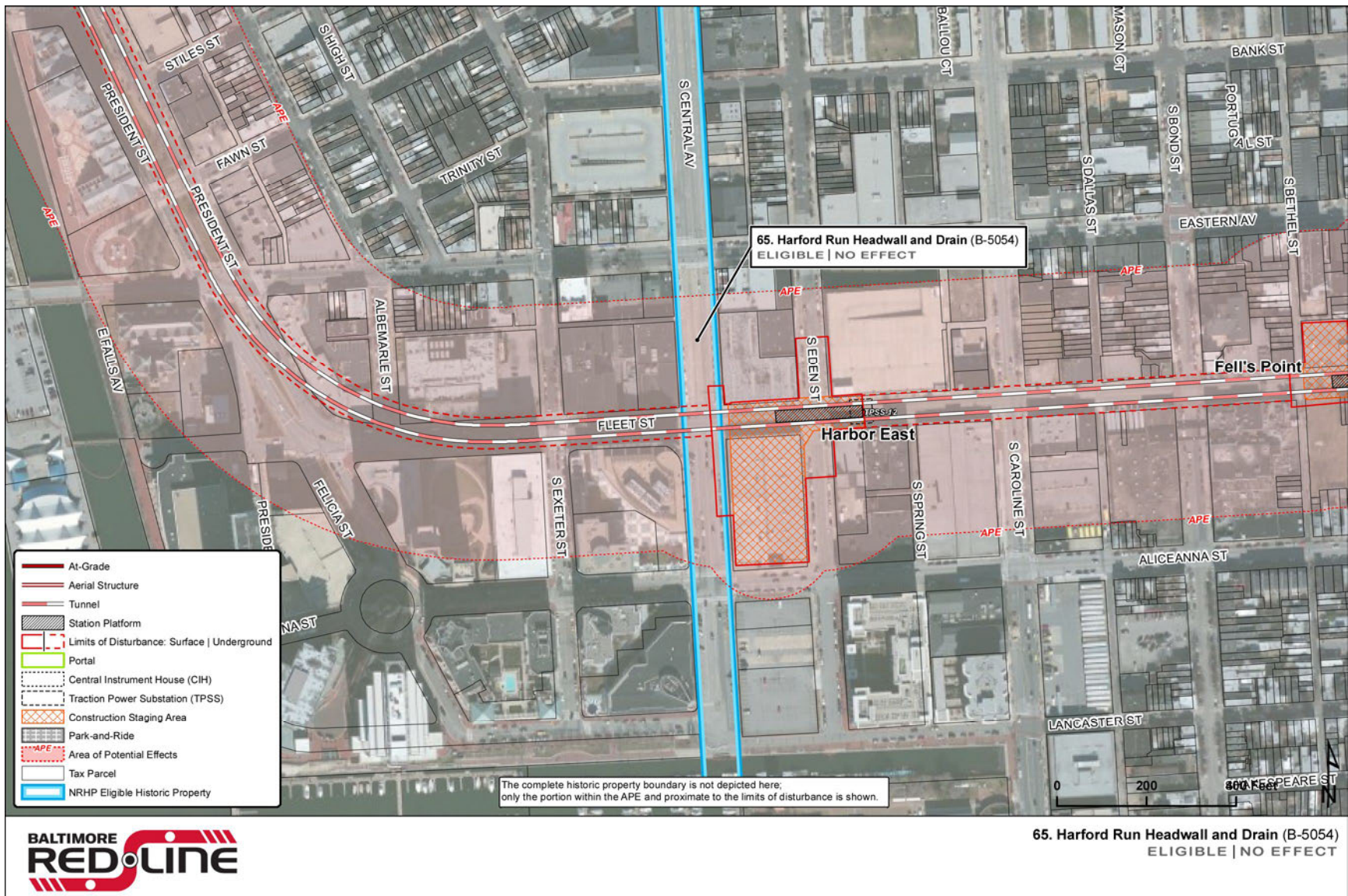


Figure 345. Proposed project in vicinity of the Harford Run Headwall and Drain



Figure 346. View northwest along South Central Avenue and above the Harford Run Headwall and Drain; proposed tunnel alignment in the foreground beneath Fleet Street



Figure 347. View southwest along South Central Avenue and above the Harford Run Headwall and Drain from the proposed tunnel alignment beneath Fleet Street



Figure 348. View southeast along Fleet Street and the proposed tunnel alignment, and toward proposed Harbor East Station and a traction power substation (all beneath Fleet Street); Harford Run Headwall and Drain in foreground (along Central Avenue); staging area would be located at warehouse to south

66. Fell's Point Historic District

See map for historic district boundaries
MIHP No. B-3714

The Fell's Point Historic District occupies approximately 75 acres in eastern Baltimore City and developed as an important early port town. Established in the 1770s, the majority of residential buildings in the district were built in the late eighteenth and early nineteenth centuries and were occupied by seamen or those working in occupations supporting seafaring activities. These houses are primarily two-and-one-half story brick row houses, with occasional three-and-one-half story houses that belonged to shipyard owners, sea captains, or successful merchants. The district's other significant buildings include warehouses, market buildings, and shops. The Fell's Point Historic District was listed in the NRHP in 1969 with a boundary increase in 1986. The district is significant under Criterion A for its association with early port and maritime activities in Baltimore. It is also significant under Criterion C for its eighteenth- and early-nineteenth-century architecture, which retains high degrees of integrity and is a significant collection of various types of buildings from this era. The historic district boundary is irregularly shaped and is depicted on the accompanying map.

Red Line Project activity within the Fell's Point Historic District would be extensive; the area where project work would occur is surrounded by contributing buildings in this resource-dense historic district. The underground alignment would extend through the district beneath Fleet Street. The underground Fell's Point Station would be located beneath the block of Fleet Street between South Bethel Street and South Broadway. The station would be constructed using cut-and-cover tunneling techniques that would adjoin the alignment's bored tunnel. Station construction would require substantial disruption on Fleet Street extending from the historic bank building on the north side of Fleet Street to the facades of the historic houses on the south side of the same street; all of these buildings are contributing elements to the Fell's Point Historic District. A fan plant/vent structure that would be four to six stories in height would be built on the bank parking lot, which is not contributing, and would not physically impact the bank building, which is contributing. The precise appearance of the fan plant, including cladding materials, has not been determined, but it would have a square footprint measuring 50 feet by 50 feet. Construction activities are anticipated to last for approximately four years; because of this time span, they would not be considered temporary impacts. Additionally, station entrance construction would occur on South Broadway's median between Fleet Street and Eastern Avenue. South Broadway would be maintained as a divided roadway and, at this time, only non-historic paving materials would be impacted. Entrance features built on this median would include escalators and canopies.

Project activities would not impact the Fell's Point Historic District's integrity of location. No effect to the district's location would occur. Physical impacts to the Fell's Point Historic District would occur; as described above, extensive project activity is proposed within the district's NRHP boundary. Adverse effects to the district's integrity of design, materials, and workmanship would occur because of the extensive construction activities within the historic district boundaries.

The integrity of setting remains high within the historic district; low-scale buildings and minimal new construction result in this integrity of setting. Building the tall fan plant/vent structure within the historic district would be obtrusive and introduce an element out of character with the historic district's intact historic setting, resulting in a visual adverse effect to the Fell's Point Historic District's setting. The station construction activities, which would be extensive and long-term, would also adversely affect the historic district's setting. Therefore, project implementation would have an adverse effect to the district's integrity of setting.

The Fell's Point Historic District retains integrity of feeling and association. Character-defining features that convey the district's expression of its aesthetic and period in time, as well as its association with Baltimore's maritime activities, are present and would be affected by the Red Line Project because of the long-term construction impacts. Lengthy project construction would substantially obscure the district's expression of its historic significance and character-defining features. Therefore the project would have an adverse effect on the Fell's Point Historic District's integrity of feeling and association.

Based on this evaluation, the Red Line Project would have an **adverse effect** to the Fell's Point Historic District.

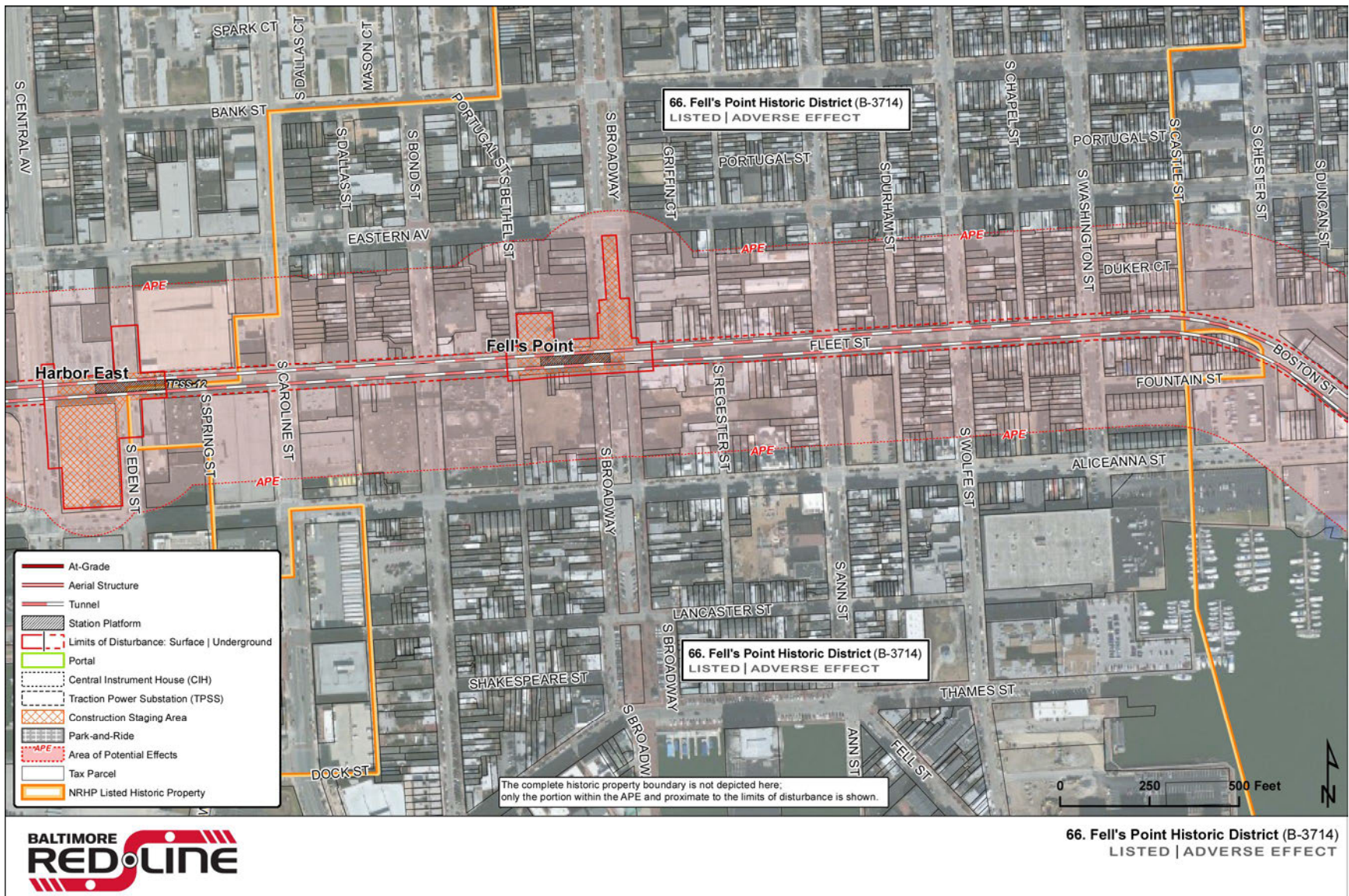


Figure 349. Proposed project in vicinity of the Fell's Point Historic District



Figure 350. View southeast along Fleet Street and the proposed tunnel alignment at South Eden Street, and to proposed Harbor East Station and a traction power substation (all beneath Fleet Street); Fell's Point Historic District visible to south



Figure 351. View northeast along the proposed tunnel alignment beneath Fleet Street at South Caroline Street and within Fell's Point Historic District



Figure 352. View northwest within the Fell's Point Historic District (at South Broadway) toward the proposed tunnel alignment and Fell's Point Station (both beneath Fleet Street); the fan plant/vent structure and a staging area would be in parking lot west of buildings to north



Figure 353. View northeast within the Fell's Point Historic District from South Broadway toward the proposed tunnel alignment and the Fell's Point Station (both beneath Fleet Street); station entrance would be within the South Broadway median



Figure 354. View southeast within the Fell's Point Historic District from South Broadway toward the proposed tunnel alignment and the Fell's Point Station (both beneath Fleet Street); station entrance would be within the South Broadway median



Figure 355. View northeast within the Fell's Point Historic District (near South Broadway) along the proposed tunnel alignment beneath Fleet Street



Figure 356. View southwest within the Fell's Point Historic District (at Washington Street) along the proposed tunnel alignment beneath Fleet Street



Figure 357. View southeast within the Fell's Point Historic District (near South Washington Street) toward the proposed tunnel alignment beneath Fleet Street

67. Douglass Place

516-524 South Dallas Street

MIHP No. B-3694

Douglass Place is a group of five, two-story, brick, Italianate-style alley row houses built by Frederick Douglass ca. 1892 in the Fells Point neighborhood as rental housing for African Americans. The buildings are typical of alley row houses of this period in Baltimore. Douglass Place was listed in the NRHP under Criterion B for its association with Frederick Douglass as the only known property in the city associated with the famed abolitionist, orator, editor, and statesman.

All Red Line Project components would be underground in Douglass Place's vicinity; no project features would be aboveground or visible. Project work would occur approximately 60 feet south of Douglass Place's historic property boundary, beneath Fleet Street.

No physical impacts to Douglass Place would occur; no project activity is proposed within the property's NRHP boundary. Therefore, no effects to the property's integrity of location, design, materials, and workmanship would occur.

Despite a non-historic parking lot located southeast of the property, Douglass Place retains integrity of setting due to its orientation to South Dallas Street, which retains its original alley row houses and streetscape. Project implementation would not affect Douglass Place's integrity of setting as all project components would be located underground and would not alter the property's visual setting. No historically significant or proximate views to or from the property would be obscured by project implementation, and no character-defining features of the property's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the property were identified. Therefore, project implementation would have no effect to Douglass Place's integrity of setting.

Furthermore, no project activity would alter the property's feeling as a group of late nineteenth-century, Italianate-style alley row houses, or its association with that style or Frederick Douglass. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to Douglass Place.

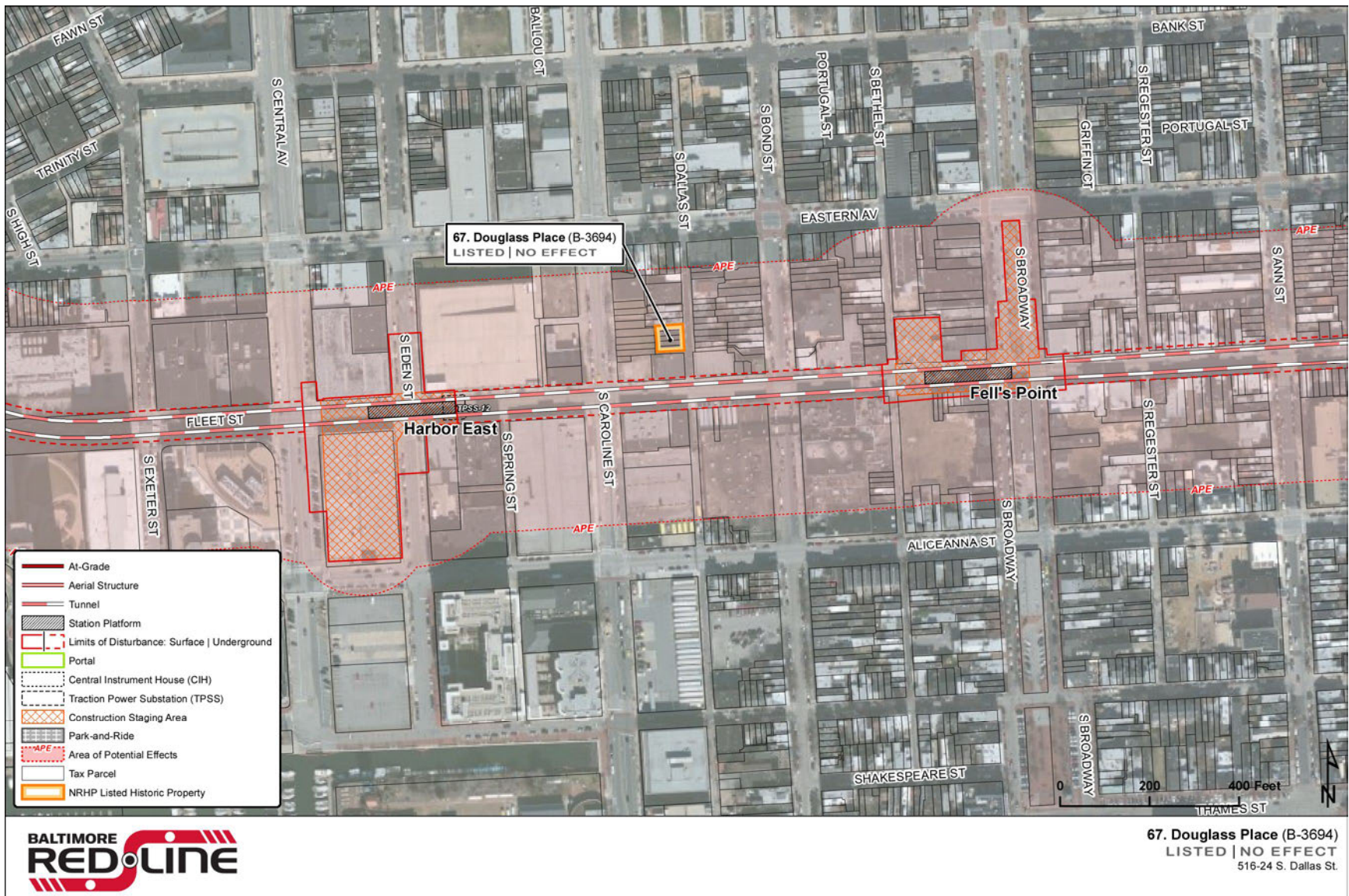


Figure 358. Proposed project in vicinity of Douglass Place



Figure 359. View northwest from proposed tunnel alignment beneath Fleet Street to South Dallas Street and Douglass Place (brick rowhouses)



Figure 360. View south from Douglass Place along South Dallas Street toward the proposed tunnel alignment beneath Fleet Street

68. Public School No. 25 (Captain Henry Fleete School)

511 South Bond Street

MIHP No. B-3928

Public School No. 25 is a late-nineteenth-century school building that provided education to the Fells Point community for nearly 75 years. Designed in the Romanesque Style, the property's most prominent feature is its imposing central tower with arched windows, defined cornice lines, and pyramidal roof. Two porches flank the central tower at street level, providing access to the school, and stone lintels and sills, projecting brickwork, and capitals with a book-and-scroll design add elements of ornamentation to the school. Public School No. 25 was listed in the NRHP under Criterion A for its association with the Fells Point community and under Criterion C for its high-style architecture used in a school building of its era. The school is also a contributing resource to the Fell's Point Historic District (B-3714).

Proposed Red Line Project construction located in the vicinity of Public School No. 25 would primarily be underground with the exception of components related to the Fell's Point Station. The proposed alignment would be located underground approximately 70 feet south of the property's south NRHP boundary and beneath Fleet Street. The proposed Fell's Point Station would be located approximately 130 feet southeast of Public School No. 25 and also underground. The station would be built using cut-and-cover construction, which would connect with bored sections of tunneling for the underground alignment. Underground station construction would occur within the Fell's Point Historic District within the street footprint on Fleet Street between Bethel Street and Broadway; construction would require up to four years and is not considered a temporary impact because of this time period's length. The station's fan plant/vent structure would be located above ground and in a parcel currently utilized as a surface parking lot; this structure would be located approximately 150 feet southeast of the historic school building. The precise materials used to clad the fan plant/vent structure have not been determined, but the structure would be between four and six stories in height with a square footprint measuring 50 feet by 50 feet.

No physical impacts to Public School No. 25 would occur; no project activity is proposed within the property's NRHP boundary. Therefore, no effects to the property's integrity of location, design, materials, and workmanship would occur.

Public School No. 25 is located within the Fell's Point Historic District, a neighborhood comprised primarily of low-rise historic buildings, and retains integrity of setting. Most buildings near Public School No. 25 are two to three stories. Red Line Project components in this property's vicinity would be primarily underground with the notable exception of the four to six-story fan plant/vent structure, which would be located approximately 150 feet southeast of the historic school building and visible from it. Construction of this large utility structure within Public School No. 25's southeast viewshed would introduce an element out of character with the property's historic setting and result in an adverse visual effect. Therefore, project implementation would have an adverse effect to Public School No. 25's integrity of setting.

No project activity would alter the property's feeling as a late-nineteenth-century, Romanesque Revival-style school building or its association with the development of Baltimore's public

school system. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have an **adverse effect** to Public School No. 25.

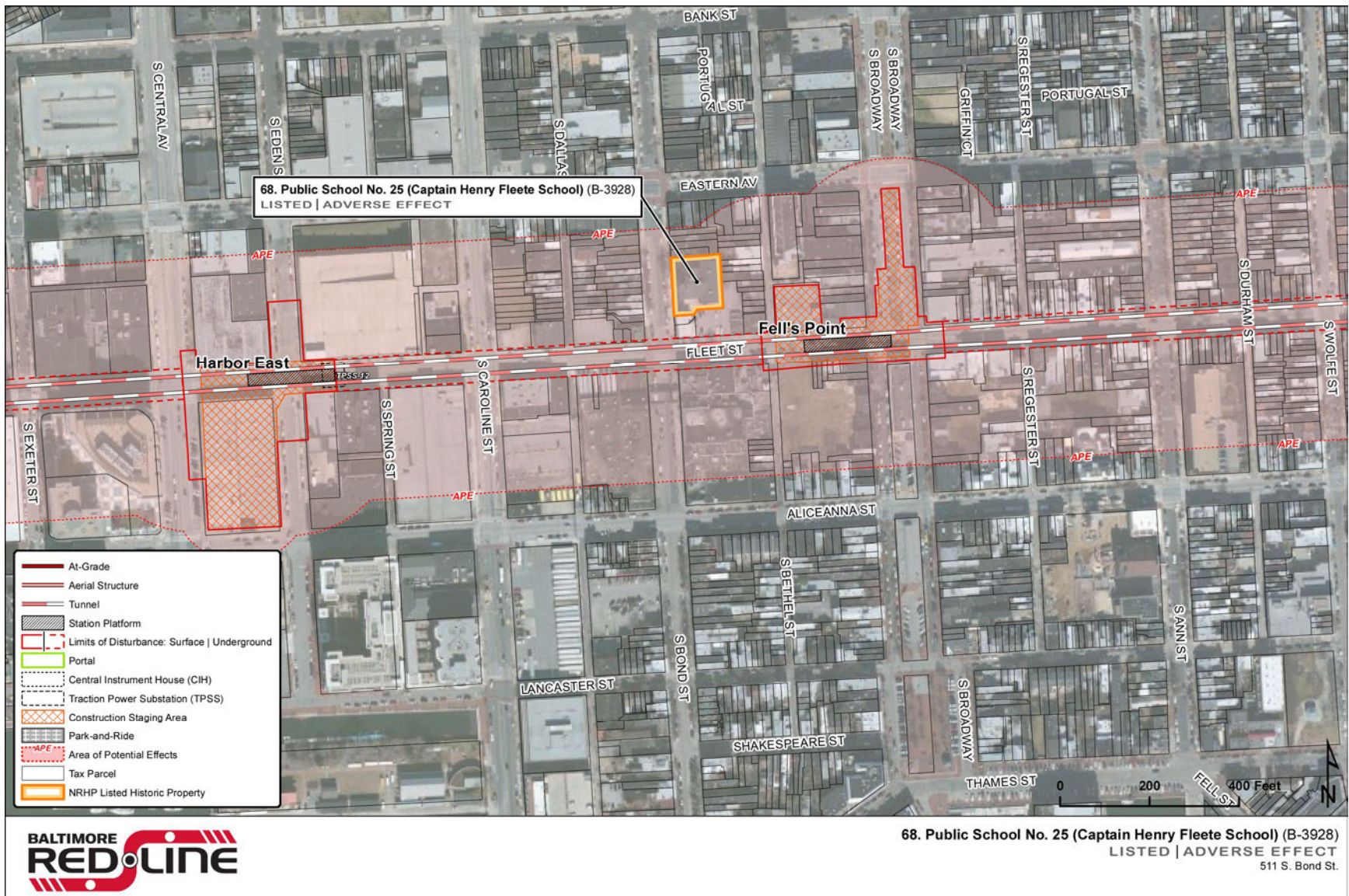


Figure 361. Proposed project in vicinity of Public School No. 25 (Captain Henry Fleete School)



Figure 362. View northeast towards the proposed tunnel alignment beneath Fleet Street at South Bethel Street and to Public School No. 25 (brick building with tower)



Figure 363. View south from Public School No. 25 along South Bond Street toward the proposed tunnel alignment beneath Fleet Street



Figure 364. View northwest from proposed tunnel alignment beneath Fleet Street to proposed fan plant/vent structure and staging area (both in visible parking lot); to South Bethel Street; and to Public School No. 25 (see red arrow)

69. Canton Historic District

Bound by South Conkling Street, Baltimore Harbor, South Chester Street, and Eastern Avenue
MIHP No. B-3704

The Canton Historic District is a historically working-class neighborhood comprised largely of brick row houses built between the mid-nineteenth and early twentieth centuries, with larger industrial buildings lining the district's southern edge. Laid out in a grid pattern, the community's development was supervised for over 100 years by the Canton Company, a real estate development firm. Several warehouses, the residential neighborhood, and social composition of Canton have retained their historic character. The Canton Historic District was listed in the NRHP under Criterion A for its association with Canton's industrial development during the mid-nineteenth to late twentieth centuries and under Criterion C as an example of a company-planned community.

Red Line Project work within the Canton Historic District would be extensive. The alignment would enter the historic district's western boundary just south of Fleet Street below ground in a tunnel. The dual-track alignment would transition to grade along Boston Street just south of South Montford Street and a portal would be located in front of contributing resources, including row houses and the American Can Company. Moving eastward, the Canton Station would be located in Boston Street's median and just west of the South Lakewood Avenue intersection, adjacent to the non-historic Safeway grocery store. The aboveground station would consist of a platform that is approximately 190 feet long and 15 feet wide with a partial canopy. The alignment would then continue to the eastern historic district boundary. Two traction power substations would be located in the Canton Historic District. One traction power substation would be located just east of the Canton station and south of the non-historic Safeway grocery store in a non-contributing parking lot. A second substation would be located near the district's southeastern corner, just north of Boston Street, and adjacent to the Kauffman Electric Company, which is not individually eligible but a contributing feature to the district; it would be located within existing right-of-way. Screening measures may be implemented for both traction power substations to minimize visual intrusions. Other project components located within the historic district include the alignment's overhead catenary system, support poles, and associated lighting.

Project work would occur within the Canton Historic District boundary but would not directly impact any contributing resources. The project would have no effect to the district's integrity of location and no adverse effects to its integrity of design, materials, and workmanship.

The Canton Historic District retains moderate integrity of setting within the district, although extensive recent development has occurred within its boundaries and changed the historic character of select areas. Original manufacturing buildings, warehouses, residences, and sacred buildings dominate the area. Although project components, including trackwork and the catenary system, would be visible from many vantage points within the district, these project facilities represent a minor alteration to the district's visual setting and would have no adverse effect to the district's setting. Likewise, the proposed station's presence in the median of Boston Street adjacent to new retail construction is not an adverse effect, nor is the presence of the traction power substations. The proposed tunnel beneath Boston Street would also not alter the district's visual setting. The proposed portal that transitions the tunnel to grade would be located

directly south of significant contributing buildings, including the American Can Company. The portal wall would be approximately three feet in height and constructed in a neutral-color concrete; a non-opaque wall would extend above the portal wall and be approximately ten feet tall. However, in this area, new construction has already compromised the historic buildings' relationship to Baltimore harbor and the integrity of setting for these particular buildings. Therefore, project implementation would have no adverse effect to the district's integrity of setting.

No project activity would alter the district's feeling as a collection of late-nineteenth- and early-twentieth-century residential, manufacturing, and sacred architecture associated with the Canton Company. Therefore, project implementation would have no effect to the district's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no adverse effect** to the Canton Historic District.

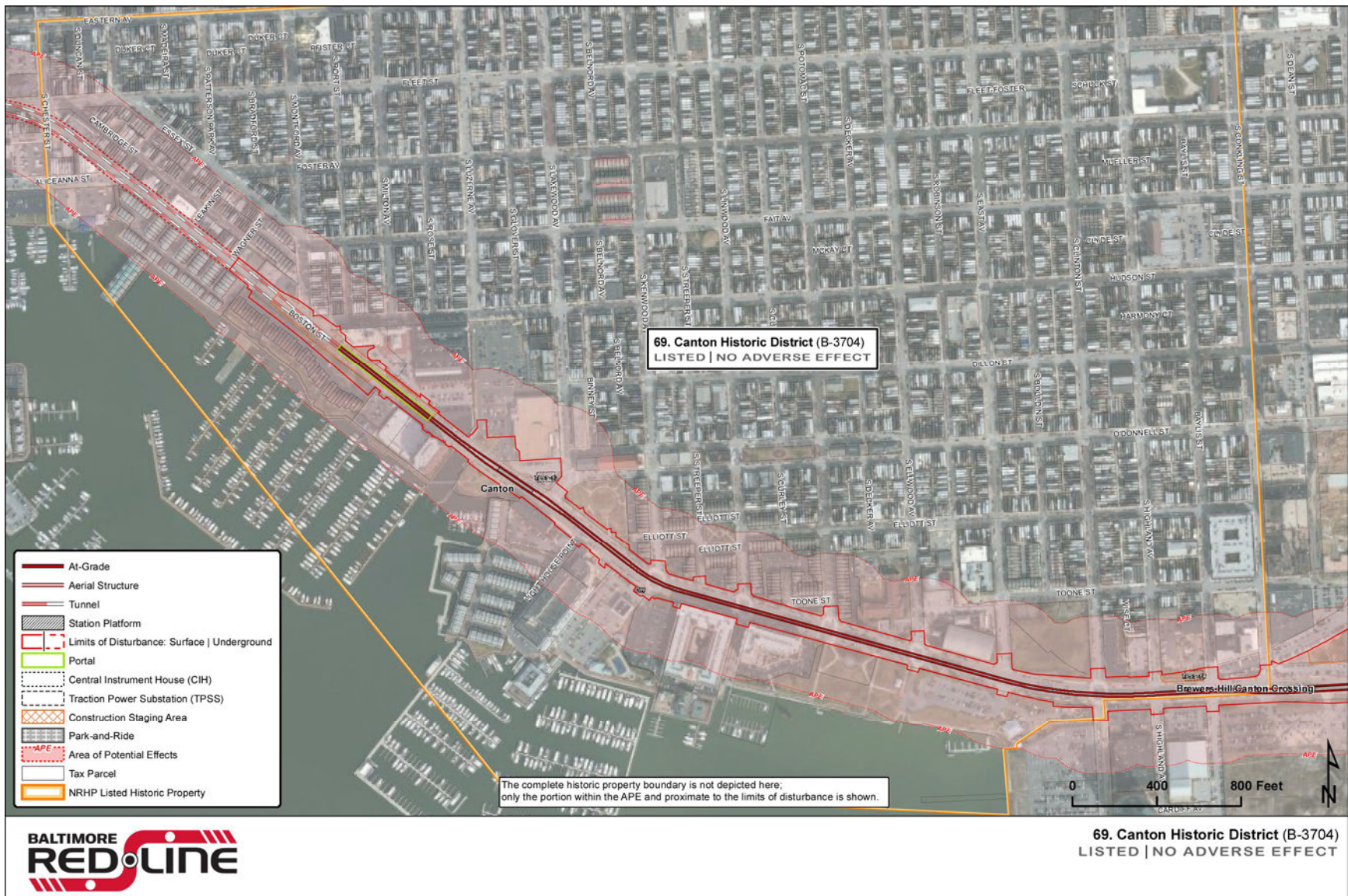


Figure 365. Proposed project in vicinity of Canton Historic District



Figure 366. View southeast within the Canton Historic District (between Leakin and Wagner streets) along proposed alignment on Boston Street and its transition from tunnel to grade



Figure 367. View southeast within the Canton Historic District along the proposed alignment on Boston Street and its transition from tunnel to grade (at South Montford Avenue)



Figure 368. View northwest within the Canton Historic District along the proposed at-grade alignment on Boston Street at South Lakewood Avenue and toward the proposed Canton Station



Figure 369. View southeast within the Canton Historic District on Boston Street along the proposed at-grade alignment and toward the proposed Canton Station and South Lakewood Avenue



Figure 370. View east from within the Canton Historic District (just north of South Lakewood Avenue) toward the proposed at-grade alignment (on Boston Street), Canton Station, and traction power substation



Figure 371. View southwest within the Canton Historic District (from South Kenwood Avenue) toward the proposed at-grade alignment on Boston Street and a proposed central instrument house



Figure 372. View southwest from within the Canton Historic District (at South Highland Avenue) toward the proposed at-grade alignment on Boston Street



Figure 373. View northwest from South Baylis Street toward the proposed at-grade alignment on Boston Street, a traction power substation and staging area (both on grassy area to north), and the Canton Historic District

70. Atlantic Southwestern Broom Company

3500 Boston Street

MIHP No. B-992

The Atlantic Southwestern Broom Company is a brick factory complex with an original four-story central core constructed in 1911 with early historic additions built in 1928 and 1934. Facing west to South Baylis Street, the original six-bay by fourteen-bay central core's symmetrical facade comprises evenly spaced fenestration; most windows have been replaced, although some original windows remain on the third and fourth stories. An industrial external chimney rises from the central core next to the coal room. The 1928 addition is located at the factory's north elevation and the 1934 addition is located at the east elevation; both match the original factory in materials, design, and style. The factory originally produced handmade brooms and in such volume that the Atlantic Broom Company (later the Atlantic-Southwestern Broom Company) became one of the largest manufacturers of handmade brooms in the country by the 1930s. The factory is now owned by the Harbor Enterprise Center and adaptively re-used for start-up company offices, artist studios, professional offices, designers, woodworkers, teaching facilities, and more. The Atlantic Southwestern Broom Company was determined eligible for listing in the NRHP under Criterion A for its association with the Atlantic Broom Company, one of the largest manufacturers of handmade brooms in the country, and under Criterion C as a good example of an intact industrial factory building. It is also located in the NRHP-listed Canton Historic District.

The Red Line Project components in the vicinity of the Atlantic Southwestern Broom Company would consist of track, overhead catenary lines, and support poles along Boston Street, approximately 115 feet south of the building's south elevation and historic property boundary. The Brewers Hill/Canton Crossing Station would also be located in Boston Street's median, approximately 310 feet southeast of the building's south elevation and historic property boundary. The station would be aboveground and consist of a 190-foot by 15-foot platform with a partial canopy. A traction power substation would be located 70 feet southwest of the historic property boundary. It would be minimally visible from the historic property boundary and screening measures may be implemented if appropriate.

No physical impacts to the Atlantic Southwestern Broom Company would occur; no project activity is proposed within the property's NRHP boundary. Therefore, no effects to the property's integrity of location, design, materials, and workmanship would occur.

The Atlantic Southwestern Broom Company does not retain integrity of setting due to the construction of nearby non-historic buildings and surface parking lots, although some original historic buildings remain intact in its vicinity. Although the proposed tracks, overhead catenary lines, transit station, and traction power substation would be visible from some portions of the building's south and east elevations, these project facilities represent a minor alteration to the property's already altered visual setting. Further, the building's facade and entrance are oriented to South Baylis Street and away from the proposed project elements. No historically significant views to or from the property remain. No visual effects to the property were identified. Therefore, project implementation would have no effect to the Atlantic Southwestern Broom Company's integrity of setting.

Furthermore, no project activity would alter the property's feeling as an early twentieth-century factory building or its association with the Atlantic & Pacific Corporation broom manufacturers. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the Atlantic Southwestern Broom Company.

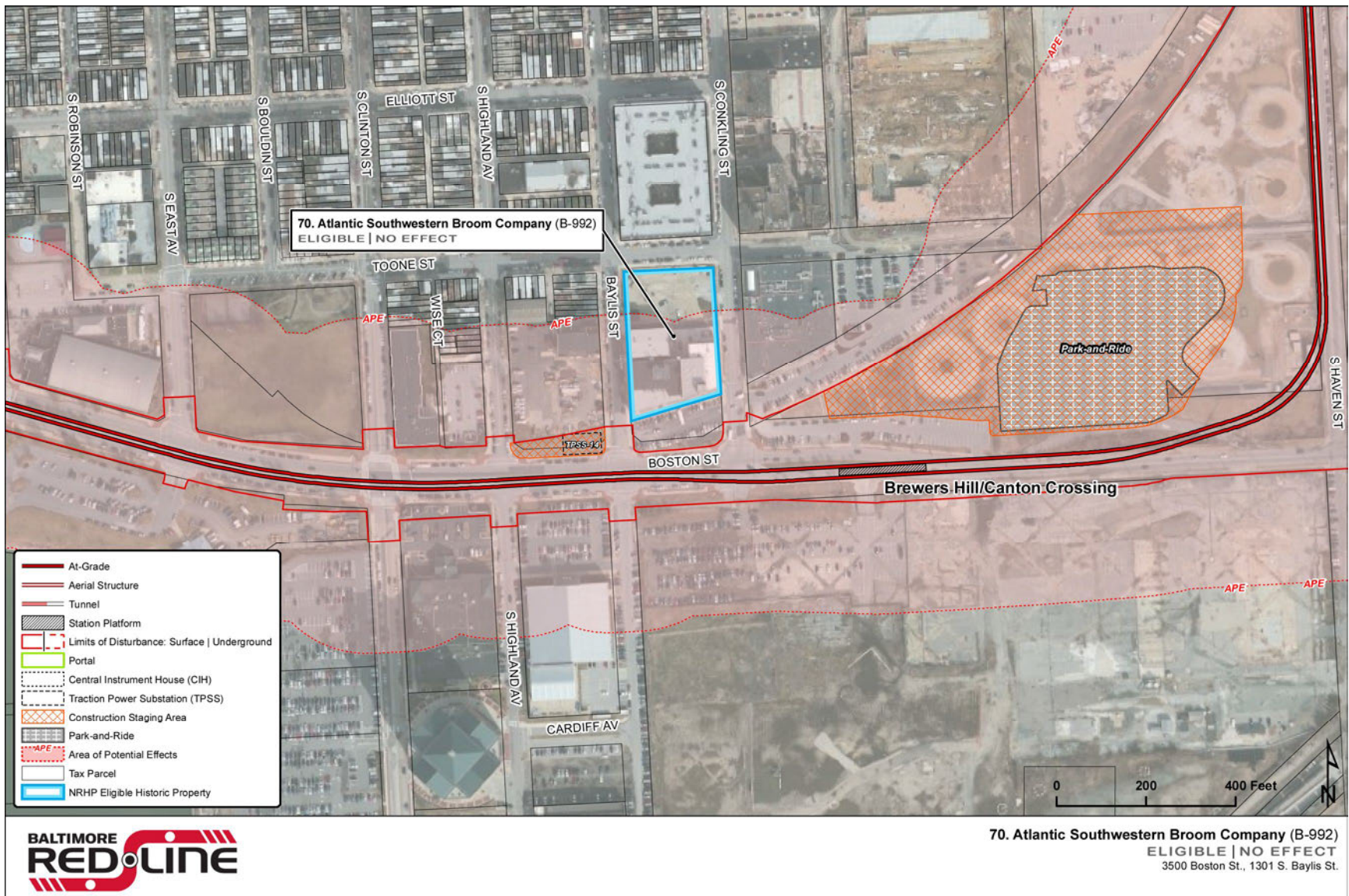


Figure 374. Proposed project in vicinity of Atlantic Southwestern Broom Company



Figure 375. View northeast toward the Atlantic Southwestern Broom Company; proposed at-grade alignment in foreground on Boston Street



Figure 376. View east along the proposed at-grade alignment on Boston Street (to the south)



Figure 377. View southwest from the Atlantic Southwestern Broom Company toward the proposed at-grade alignment (on Boston Street), and to a proposed traction power substation and staging area (both on grassy area to west)



Figure 378. View southeast from the Atlantic Southwestern Broom Company parking lot toward the proposed at-grade alignment on Boston Street

71. Gunther Brewing Company (Hamm's Brewing Company)

1201-1301 South Conkling Street and 3601-3701 O'Donnell Street
MIHP No. B-998

The Gunther Brewing Company (Ham's Brewing Company) occupies a site that contains fifteen masonry buildings constructed between 1900 and 1963. The most notable built features include a Romanesque Revival-style building directly related to creating beer. These include a brew house, ice plant, and bottling building. The complex as a whole demonstrates changes in brewing methodology and provides insight into brewing history, technology, and operations in Baltimore from 1900 until 1978, when brewing ceased. The Gunther Brewing Company (Hamm's Brewing Company) was listed in the NRHP in 2002. It is significant under Criterion A for its association with Baltimore's brewing history and industry in the Brewer's Hill/Canton area, and for demonstrating the evolution of brewing as a business during the twentieth century. The NRHP boundary is bordered by O'Donnell and Conkling streets; large, non-historic, asphalt-paved parking lots are located within the northeast NRHP boundary.

Red Line Project activity near the Gunther Brewing Company consists of the construction of the alignment south and east of the property's NRHP boundary and the above-ground Brewers Hill/Canton Crossing Station that would be built approximately 260 feet south of the closest brewery building and in the Boston Street median. The station would consist of a platform that is approximately 190 feet long and 15 feet wide with a partial canopy. Other proximate project components would include trackwork, overhead catenary lines, and support poles, some with lighting, along Boston Street to the property's south and South Haven Street to the property's east. A traction power substation would be located approximately 345 feet to the southwest of the historic property boundary. It would be minimally visible from the historic property boundary and screening measures may be implemented if appropriate. A park-and-ride lot is also planned approximately 140 feet southwest of the NRHP boundary; temporary construction staging would also occur in this area.

No physical impacts to the Gunther Brewing Company would occur; no project activity is proposed within the property's NRHP boundary. Therefore, no effects to the property's integrity of location, design, materials, or workmanship would occur.

The Gunther Brewing Company's integrity of setting has been diminished over time by surrounding demolitions, new construction, and alterations to existing buildings, although the industrial character of the area is retained. Locating a station in Boston Street's median and a traction power substation in that street's right of way, as well as adding trackwork, poles, and catenary lines on busy local roadways and a park-and-ride lot would have no effect on the property's setting, which has been compromised by new construction and is not a character-defining feature contributing to the Gunther Brewing Company's significance. Therefore, project implementation would have no effect to the Gunther Brewing Company's integrity of setting.

The Gunther Brewing Company retains integrity of feeling and association. Character-defining features that convey the complex's expression of its aesthetic and period in time, as well as its association with Baltimore's brewing history, are present and would not be affected by the Red

Line Project. The project would have no effect on the property's integrity of feeling and association.

Based on this evaluation, the Red Line Project would have **no effect** to the Gunther Brewing Company (Hamm's Brewing Company).

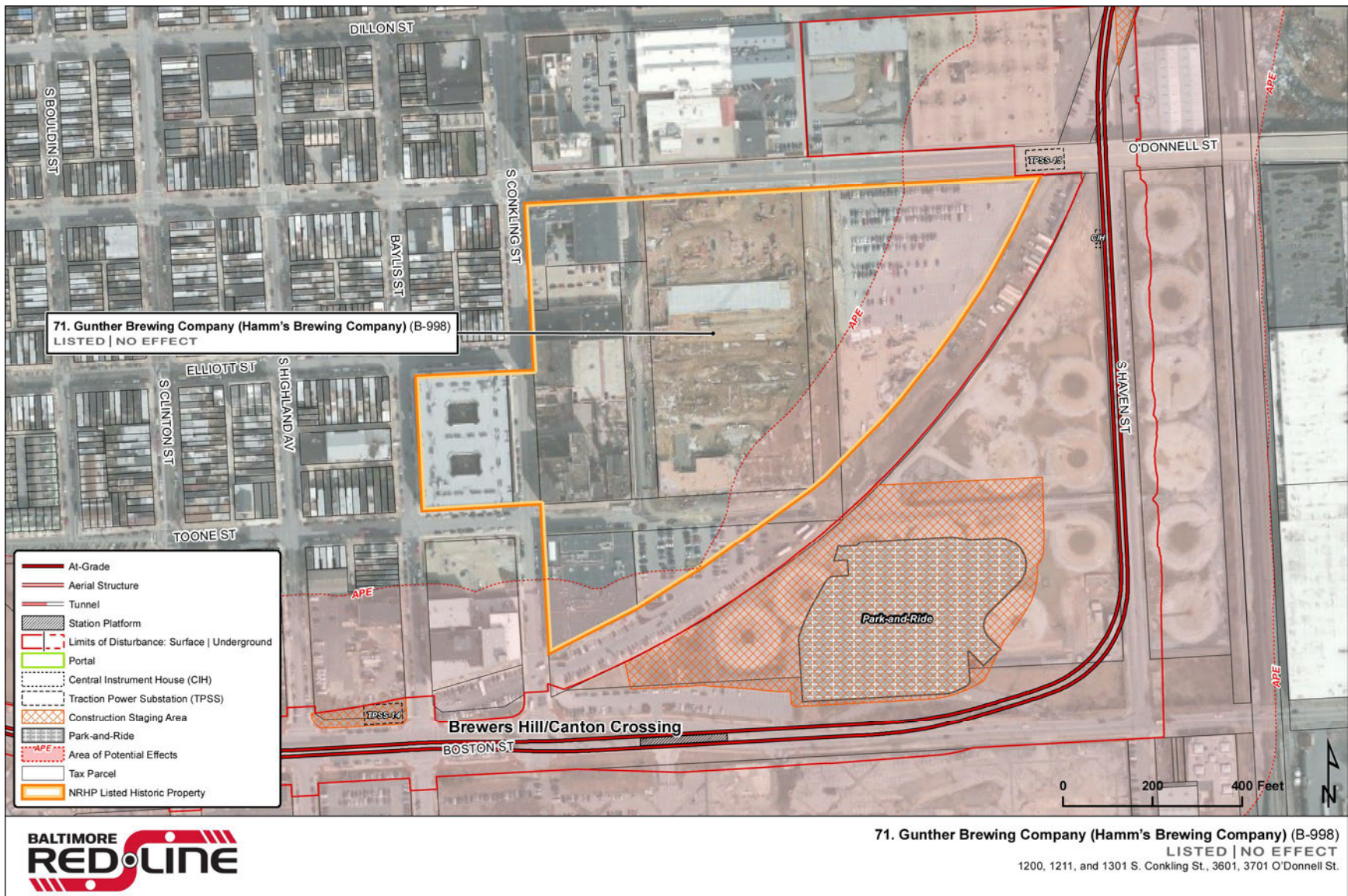


Figure 379. Proposed project in vicinity of the Gunther Brewing Company (Hamm's Brewing Company)



Figure 380. View northeast toward the Gunther Brewing Company (at South Conkling Street) with the proposed at-grade alignment on Boston Street in foreground



Figure 381. View southeast from the Gunther Brewing Company toward the proposed at-grade alignment (transitioning from Boston Street to South Haven Street), the Brewers Hill/Canton Crossing Station, a park-and-ride lot, and a staging area



Figure 382. View east from within the Gunther Brewing Company (at Toone Street) toward the proposed at-grade alignment on South Haven Street

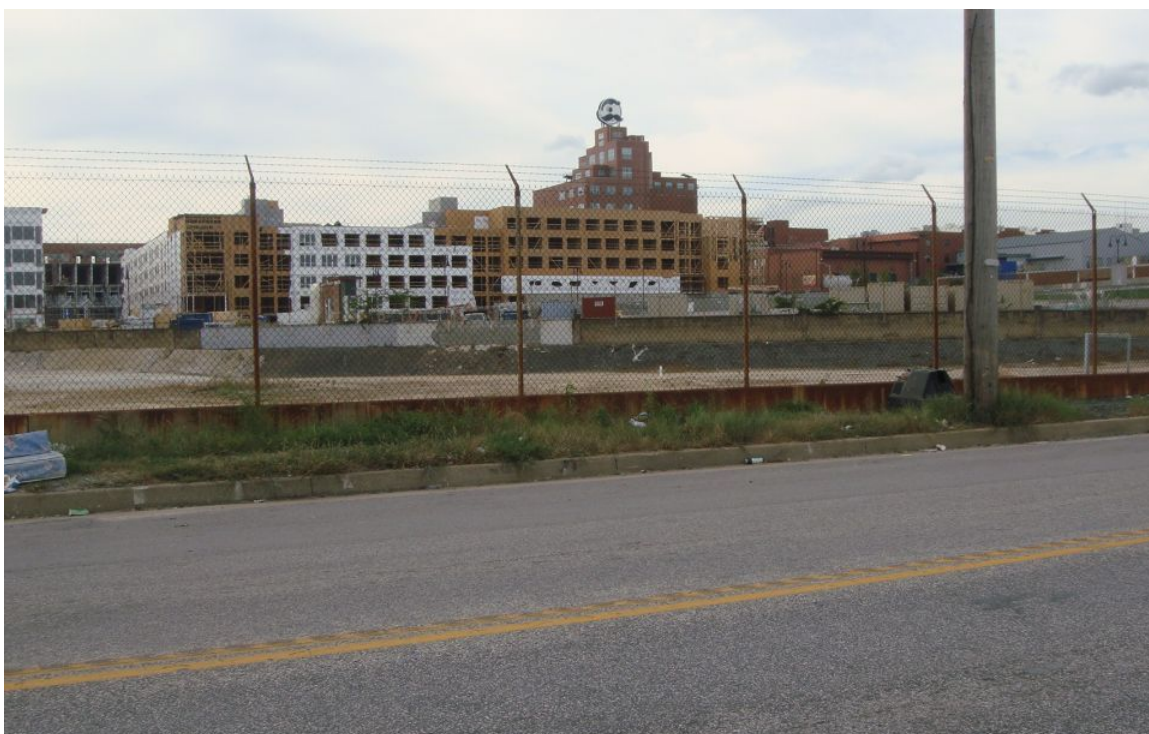


Figure 383. View northwest toward the Gunther Brewing Company from the proposed at-grade alignment on South Haven Street near Boston Street



Figure 384. View west toward the Gunther Brewing Company and a proposed central instrument house from proposed at-grade alignment on South Haven Street near O'Donnell Street



Figure 385. View southeast along O'Donnell Street from South Conkling Street toward the Gunther Brewing Company (on south); O'Donnell Street continues and bridges over South Haven Street, the proposed at-grade alignment, and a proposed traction power substation



Figure 386. View southwest from South Haven Street toward a proposed traction power substation location under the O'Donnell Street bridge; Gunther Brewing Company in the background

72. The National Brewing Company

3601-3901 Dillon Street

MIHP No. B-996

The National Brewing Company is a brewery complex comprised of eleven masonry buildings and one corrugated metal building; it includes a stock house, grain storage building, oil tank building, bottling plants, and three beer cellars. The buildings were constructed from 1885 to 1966 and vary in style from the Romanesque Revival-style 1885-1889 Beer Cellar building to brick industrial buildings that incorporate minimal decorative brickwork and stone belt courses. The stock house dominates the property with a nine-story, brick and limestone-clad center tower flanked by seven-story, brick-clad wings. This building is distinguished by full-height piers topped by roundels, a molded cornice over the second story, and belt courses above the top stories. The complex represents the evolution of a once major Baltimore industry closely linked to the city's dominant German population. It also illustrates how the brewing industry evolved through the changing technologies that characterized the pre- and post-Prohibition eras. The company was the largest brewery in Maryland by 1954 and one of the twenty largest in the nation; the brewery closed in 1970. The National Brewing Company was listed in the NRHP under Criterion A for its association with and importance to the brewing industry in Baltimore and Maryland. A 1960 annex and 1966 warehouse building are located within the NRHP boundary but were previously determined non-contributing features to the property's historic significance. The 1960 annex is located in the complex's central section between the 1933 and 1939 bottling plants, and the 1966 warehouse is located in the complex's eastern section, near South Haven Street.

The Red Line Project components in the vicinity of The National Brewing Company would consist of track and overhead catenary lines and support poles running north-south on South Haven Street, along the property's east NRHP boundary and approximately 90 feet east of the property's nearest building, the non-contributing 1966 warehouse building. A traction power substation would be located adjacent to the property's southeastern NRHP boundary; screening measures may be implemented to minimize any potential visual impacts, if appropriate. No proposed stations are proximate to The National Brewing Company.

The project's limits of disturbance would enter the east NRHP boundary and extend into the property, up to approximately 650 feet to encompass a paved parking lot and non-historic buildings for use as temporary construction storage facilities. The parking lot was part of a now abandoned and destroyed section of the Philadelphia Wilmington & Baltimore Railroad line that crossed the property at this location, and buildings proposed for temporary storage use are non-contributing. No project activity would cause physical impacts to The National Brewing Company. No historically significant buildings are located in this part of the NRHP boundary, and none are proposed for alteration or removal. No character-defining features of the property would be impacted. Therefore, no adverse effects to the property's integrity of location, design, materials, and workmanship would occur.

The National Brewing Company does not retain integrity of setting due to the construction of an overpass on O'Donnell Street along the district's south boundary and surface parking lots in its vicinity, although some original historic buildings remain intact in its viewshed. Although the

proposed tracks and overhead catenary lines would be visible from some buildings within the district to the south, west, and north, these project facilities represent a minor change to the district's altered visual setting. In addition, the district's contributing buildings are located approximately 460 feet west or further from the proposed alignment, and they are screened from it by the non-contributing 1966 warehouse building which is nearest the tracks and overhead catenary lines. The non-contributing warehouse also screens all views to the traction power substation which would be located at the property's southeastern NRHP boundary. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the property were identified. Therefore, project implementation would have no adverse effect to The National Brewing Company's integrity of setting.

Furthermore, no project activity would alter the district's feeling as a late nineteenth- and twentieth-century brewery complex or its association with the evolution of the brewing industry in Baltimore and Maryland. Therefore, project implementation would have no effect to the district's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no adverse effect** to The National Brewing Company.

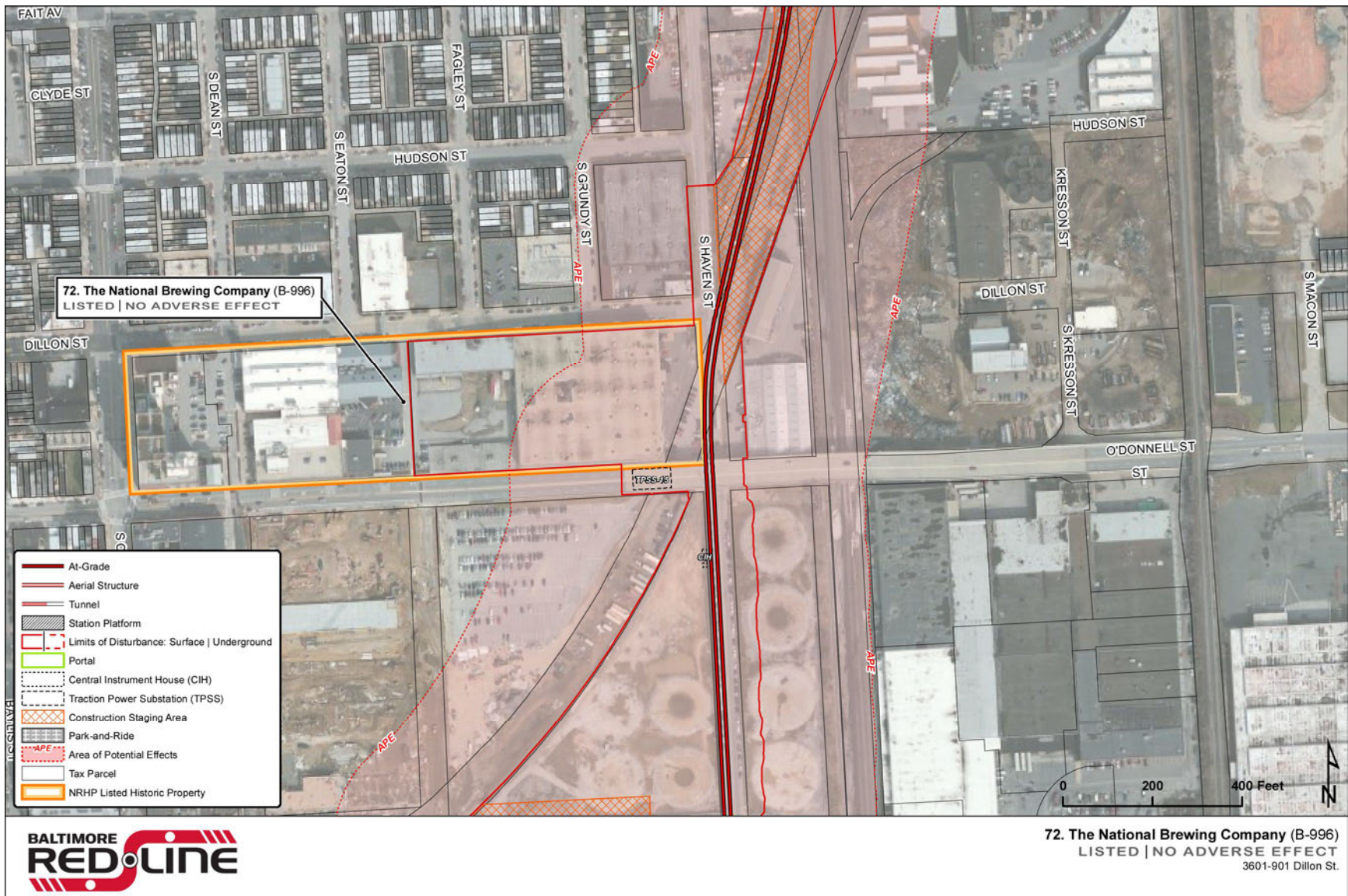


Figure 387. Proposed project in vicinity of The National Brewing Company



Figure 388. View northwest toward The National Brewing Company from the proposed at-grade alignment on South Haven Street just south of O'Donnell Street



Figure 389. View northwest toward a proposed central instrument house, the O'Donnell Street bridge, and The National Brewing Company from the proposed at-grade alignment on South Haven Street



Figure 390. View northeast along O'Donnell Street from South Conkling Street toward The National Brewing Company (on north); O'Donnell Street continues and bridges over South Haven Street and the proposed at-grade alignment and a traction power substation



Figure 391. View northwest from South Haven Street toward a proposed traction power substation location beneath the O'Donnell Street bridge; The National Brewing Company to north



Figure 392. View south along the proposed at-grade alignment on South Haven Street; The National Brewing Company to west



Figure 393. View northeast along the proposed at-grade alignment (flanked by staging areas) at its transition from South Haven Street to the Union Railroad; The National Brewing Company in west foreground



Figure 394. View east along Dillon Street and toward The National Brewing Company from the proposed at-grade alignment at its transition from South Haven Street to the Union Railroad

73. Union Railroad

Running north-south approximately 235 feet east of South Haven Street, from Boston Street to Pulaski Highway

MIHP No. B-5163

The Union Railroad consists of nine miles of rail constructed in the mid-to late nineteenth century. Its construction fulfilled a need by the Northern Central Railroad, a Pennsylvania Railroad company, for direct access to the Canton shipping wharves. The railroad allowed the Pennsylvania Railroad to compete with the Baltimore & Ohio Railroad and gave the Pennsylvania Railroad two lines feeding into Baltimore. With improved access to the shipping wharves, the Canton area grew to include warehouses and nearby factory worker residences. The Union Railroad was determined eligible for listing in the NRHP under Criterion A for its association with Canton's growth and development and the early development of the nation's railroad system.

Red Line Project work involving the Union Railroad would consist of alignment running along the historic rail alignment from approximately Foster Avenue to East Pratt Street and include replacement of rails, ties, and ballast due to their poor condition. The Red Line would also run on the elevated alignment on an existing rail bridge, a contributing resource, that passes over Eastern Avenue. The aboveground, split-platform Highlandtown/Greektown Station, which would consist of two platforms approximately 190 feet long and 15 feet wide with a partial canopy, would also be located along the Union Railroad, approximately between Fleet Street and Eastern Avenue. Construction staging areas would also encompass portions of the railroad's alignment. A traction power substation would be located beneath O'Donnell Street to the west of the rail line, west of the intersection with South Haven Street. As a noise and vibration generator, the railroad is not subject to noise and vibration impacts.

Although project work would occur along the Union Railroad, within its historic boundary, and include rail material replacement for safety purposes, the Red Line project's use of the historic alignment for continued rail purposes/service would be a compatible use that would ensure the line does not become obsolete; this continued use would include both the rail alignment and the rail bridge.

Proposed project work would have no effect to the property's integrity of location and no adverse effect to its integrity of design, materials, or workmanship. Replacing sections of the rail line with like materials, including rails, ties, and ballast, as needed, is a routine practice for safety and good maintenance practice.

The Union Railroad retains moderate integrity of setting. Select historic buildings remain in the vicinity, but many buildings have been substantially altered, demolished, or replaced with new construction. Although project elements, including the station and the traction power substation, would be visible from some portions of the railroad, these facilities represent a minor change to the property's visual setting. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property's setting would be impacted. Because no historically significant views would be obscured, no visual

effects to the property were identified. Therefore, project implementation would have no adverse effect on the Union Railroad's integrity of setting.

The Union Railroad retains integrity of feeling and association. Character-defining features that convey the rail line's engineering technology and period in time, as well as its association with transportation in Baltimore, are present and would not be affected by the Red Line Project. Therefore, the project will have no effect on the Union Railroad's integrity of feeling and association.

Based on this evaluation, the Red Line Project would have **no adverse effect** to the Union Railroad.

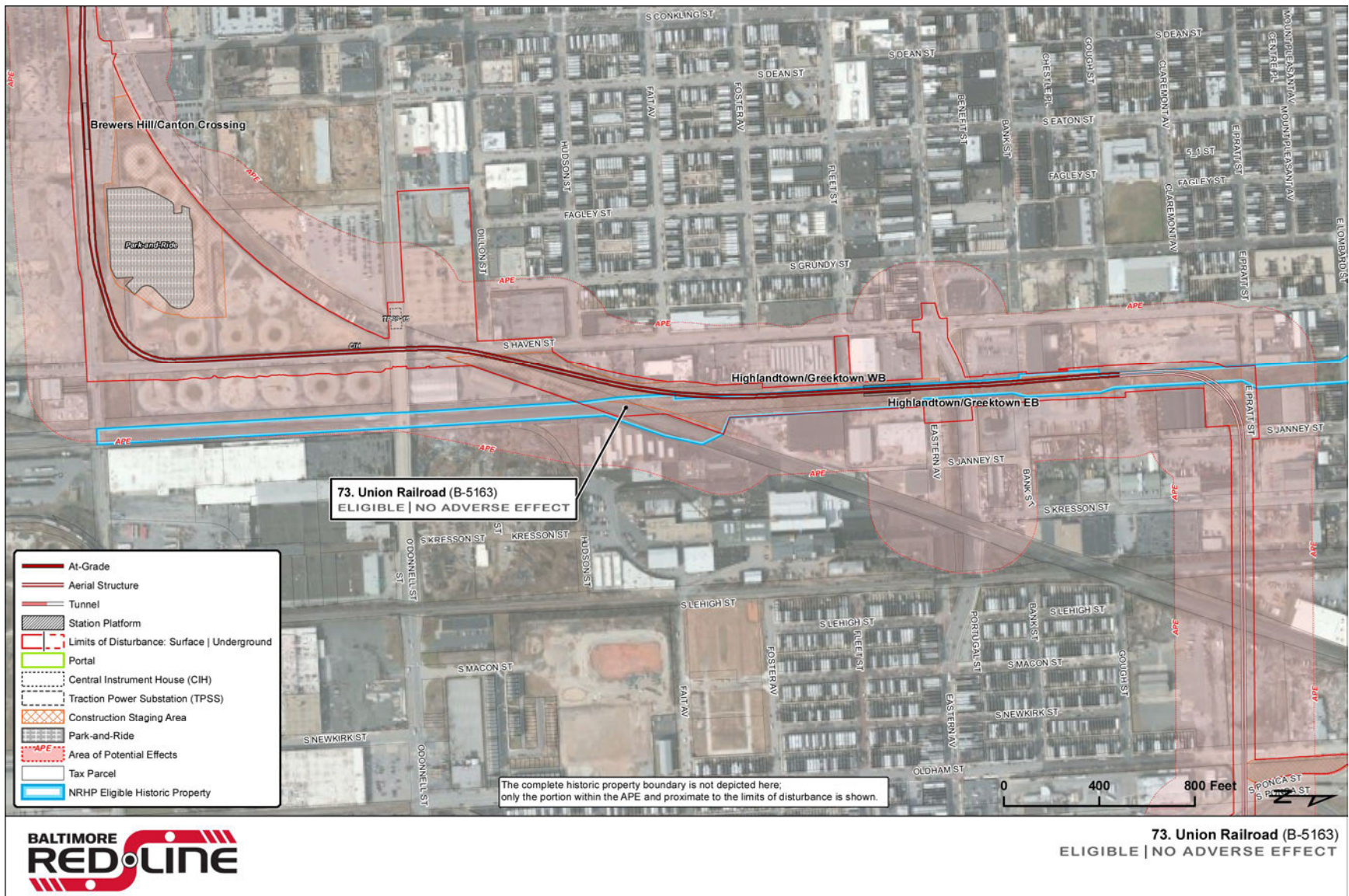


Figure 395. Proposed project in vicinity of the Union Railroad



Figure 396. View northeast toward the proposed at-grade alignment on South Haven Street (between O'Donnell and Boston streets); Union Railroad beyond trees to east



Figure 397. View southwest from Union Railroad (at O'Donnell Street) with the proposed at-grade alignment on South Haven Street beyond trees



Figure 398. View southwest from Old Eastern Avenue toward the proposed at-grade alignment and Highlandtown/Greektown Station platforms on Union Railroad



Figure 399. View west from Old Eastern Avenue to proposed at-grade alignment on Union Railroad



Figure 400. View south along proposed alignment from the Union Railroad bridge over the Eastern Avenue Underpass



Figure 401. View north along proposed alignment from the Union Railroad bridge over the Eastern Avenue Underpass



Figure 402. View east toward the alignment and Union Railroad bridge over Eastern Avenue Underpass

74. Crown Cork & Seal Highlandtown Plant Complex

4401 Eastern Avenue

MIHP No. B-5172

The Crown Cork & Seal Highlandtown Plant Complex is a collection of industrial and utilitarian buildings, both single story and multistory, and ranging in date from 1905 to 1941. A large and intact pre-World War II industrial complex, the property's main Tudor Revival-style, brick building fronts Eastern Avenue, while other buildings consist of simple concrete-and-brick construction with flat roofs and limited Italianate and Art Deco stylistic details. The complex contained laboratories that developed innovative new technologies involving cork, metal, and glass products; the company had a worldwide impact on the development of caps and containers. The Crown Cork & Seal Highlandtown Plant Complex was determined eligible for listing in the NRHP under Criterion A for its association with important innovations and manufacturing processes, and under Criterion C as an intact, pre-World War II industrial complex.

Near the Crown Cork & Seal Highlandtown Plant Complex, Red Line Project implementation would include installation of the alignment in the vicinity of the property's western NRHP boundary. The alignment would be installed along South Haven Street, to the property's southwest, and would continue northeast to within approximately 130 feet of the property's western NRHP boundary, and then turn north and run parallel and to the east of South Haven Street in the former Union Railroad right-of-way. The alignment, consisting of dual tracks and the catenary system, including support poles, wires, and some lighting, would be installed within the South Haven Street right-of-way and in the abandoned Union Railroad right-of-way. A traction power substation would be located approximately 435 feet west of the property's southwestern NRHP boundary and beneath the O'Donnell Street bridge over South Haven Street. Screening measures may be implemented to minimize any potential visual impacts, if appropriate. The proposed split-platform Highlandtown/Greektown Station, consisting of two platforms 190 feet long and 15 feet wide with partial canopy coverage, would be located approximately 340 feet from the property's northwest NRHP boundary.

No physical impacts to the Crown Cork & Seal Highlandtown Plant Complex would occur; no project activity is proposed within the property's NRHP boundary. Therefore, no effects to the property's integrity of location, design, materials, and workmanship would occur.

Project implementation would not affect the Crown Cork & Seal Highlandtown Plant Complex's setting. Although the proposed alignment and overhead catenary system would be visible from the property, these project facilities represent a minor alteration to the property's overall visual setting. The area surrounding the property is a developed industrial area with several railroad lines currently bisecting the area, and intervening buildings would partially screen views toward the alignment and the proposed station. The proposed traction power substation would be screened entirely from view. The Union Railroad historically traveled through this area via right-of-way proposed for the project alignment; rail components have been historically utilized at this location. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property's setting would be affected. Because no views would be obscured, no visual effects to the property were identified. Therefore, project implementation would have no effect to the property's integrity of setting.

Furthermore, no project activity would alter the property's feeling as a pre-World War II industrial complex, or its association with manufacturing innovation during that period. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no effect** to the Crown Cork & Seal Highlandtown Plant Complex.

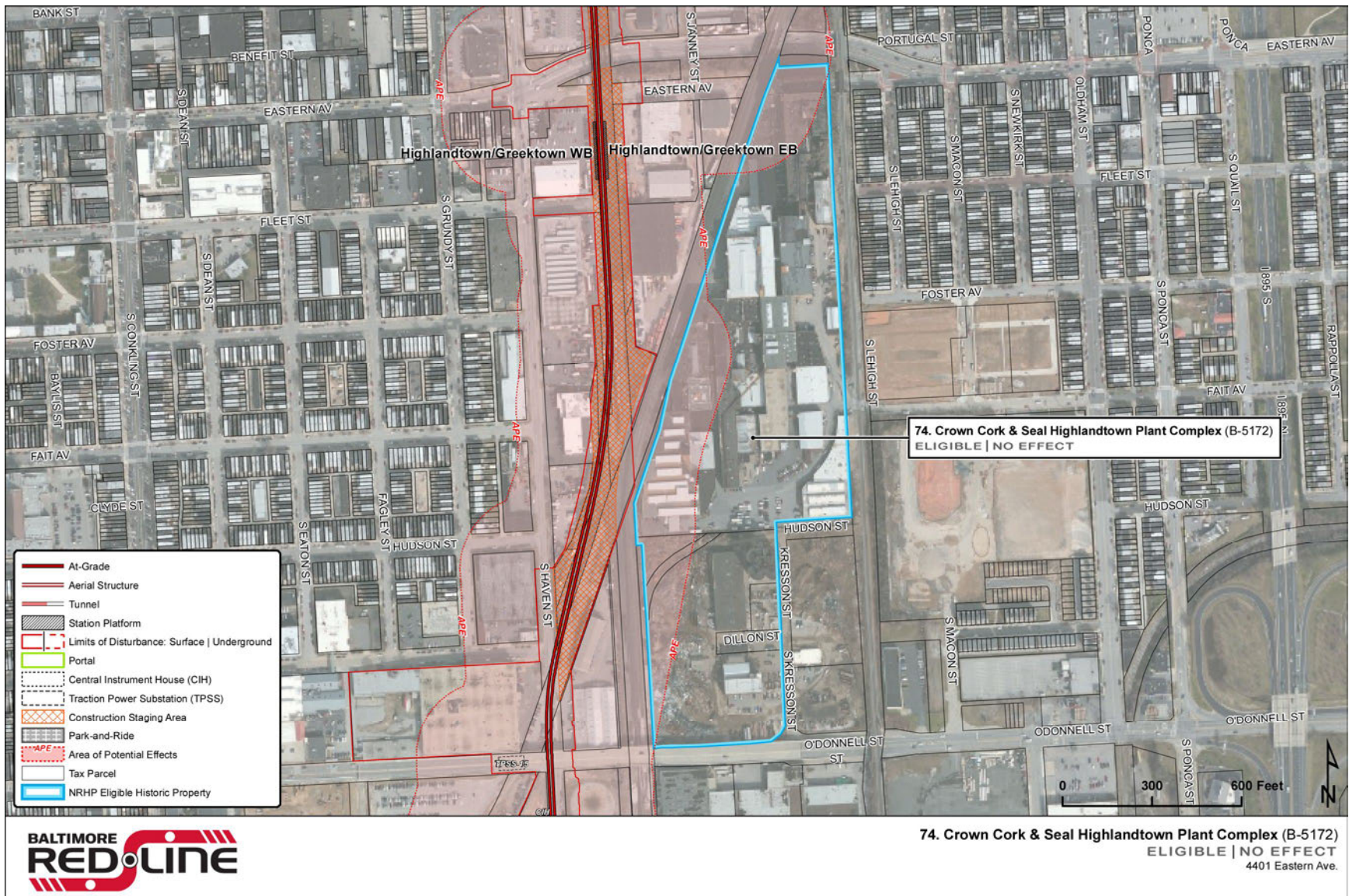


Figure 403. Proposed project in vicinity of the Crown Cork & Seal Highlandtown Plant Complex



Figure 404. View northeast toward the proposed at-grade alignment on South Haven Street (between O'Donnell and Boston streets) and to the Crown Cork & Seal Highlandtown Plant Complex in distance (including the silos)



Figure 405. View northeast toward the Crown Cork & Seal Highlandtown Plant Complex from the proposed at-grade alignment at its transition from South Haven Street to Union Railroad



Figure 406. View east along Old Eastern Avenue toward the Crown Cork & Seal Highlandtown Plant Complex from the proposed at-grade alignment on Union Railroad



Figure 407. View west along Old Eastern Avenue toward the proposed at-grade alignment on Union Railroad from the Crown Cork & Seal Highlandtown Plant Complex



Figure 408. View east toward the proposed alignment on Union Railroad bridge over Eastern Avenue Underpass and to the Crown Cork & Seal Highlandtown Plant Complex (see red arrow)

75. B & O Railroad-Philadelphia Branch

Runs north from O'Donnell Street to wye west of Bayview rail yard
MIHP No. B-5168

The B & O Railroad-Philadelphia Branch is a railroad line originally constructed in 1883 following Pennsylvania Railroad's acquisition of the Philadelphia, Wilmington & Baltimore Railroad, forcing the Baltimore & Ohio Railroad to construct its own line to the northeast for commercial and passenger use. Until that time, the Baltimore & Ohio Railroad had negotiated permission to use the Philadelphia, Wilmington & Baltimore Railroad's tracks. The Philadelphia Branch was an important link between the Canton shipping terminal and the Northeast Corridor, a rail line providing passenger, commuter, and freight services from Boston to Washington, D.C. The railroad remains in use today for freight traffic and, in the Red Line Project area, retains three bridges, various retaining walls, and other character-defining elements original to the line. The B & O Railroad-Philadelphia Branch was determined eligible for listing in the NRHP under Criterion A for its association with the industrialization of Baltimore's Canton neighborhood in the nineteenth century and under Criterion C as an example of an 1880s railroad alignment containing original character-defining features.

Near the B & O Railroad-Philadelphia Branch, Red Line Project implementation would include the installation of the alignment along the former Union Railroad right-of-way, running north-south parallel to the property's western NRHP boundary at an approximate 760-foot offset. At approximately 660 feet south of East Lombard Street, the alignment would turn east to run parallel to and approximately 430 feet south of East Lombard Street, while ascending from grade to an elevated track. The alignment, consisting of dual tracks and the overhead catenary system, including support poles, wire, and some lighting, would be installed along South Haven Street, the former Union Railroad right-of-way, and the elevated alignment while crossing over and bisecting a section of the B & O Railroad-Philadelphia Branch approximately 195 feet east of South Kresson Street. The proposed split-platform Highlandtown/Greektown Station, consisting of two platforms 190 feet long and 15 feet wide with a partial canopy, is located within the former Union Railroad right-of-way, just south of Eastern Avenue and approximately 750 feet west of the B & O Railroad-Philadelphia Branch. As a noise and vibration generator, the railroad is not subject to noise and vibration impacts.

Although the alignment segment would bisect a portion of the railroad's NRHP boundary, no physical impacts to the B & O Railroad-Philadelphia Branch would occur as the alignment would be elevated above the railroad. Therefore, no adverse effects to the property's integrity of location, design, materials, and workmanship would occur.

The B & O Railroad-Philadelphia Branch does not retain integrity of setting in the project area due to non-historic industrial development in its vicinity. Although the elevated alignment would be visible from some portions of the railroad, these facilities represent a minor change to the property's altered visual setting. To the railroad's west, numerous intervening buildings screen views to and from portions of the proposed alignment and station. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the property were identified.

Therefore, project implementation would have no adverse effect to the B & O Railroad-Philadelphia Branch's integrity of setting.

Furthermore, no project activity would alter the property's feeling as a historic railroad corridor, or its association with the industrialization of the Canton neighborhood and with Baltimore's transportation and commercial history. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no adverse effect** to the B & O Railroad-Philadelphia Branch.

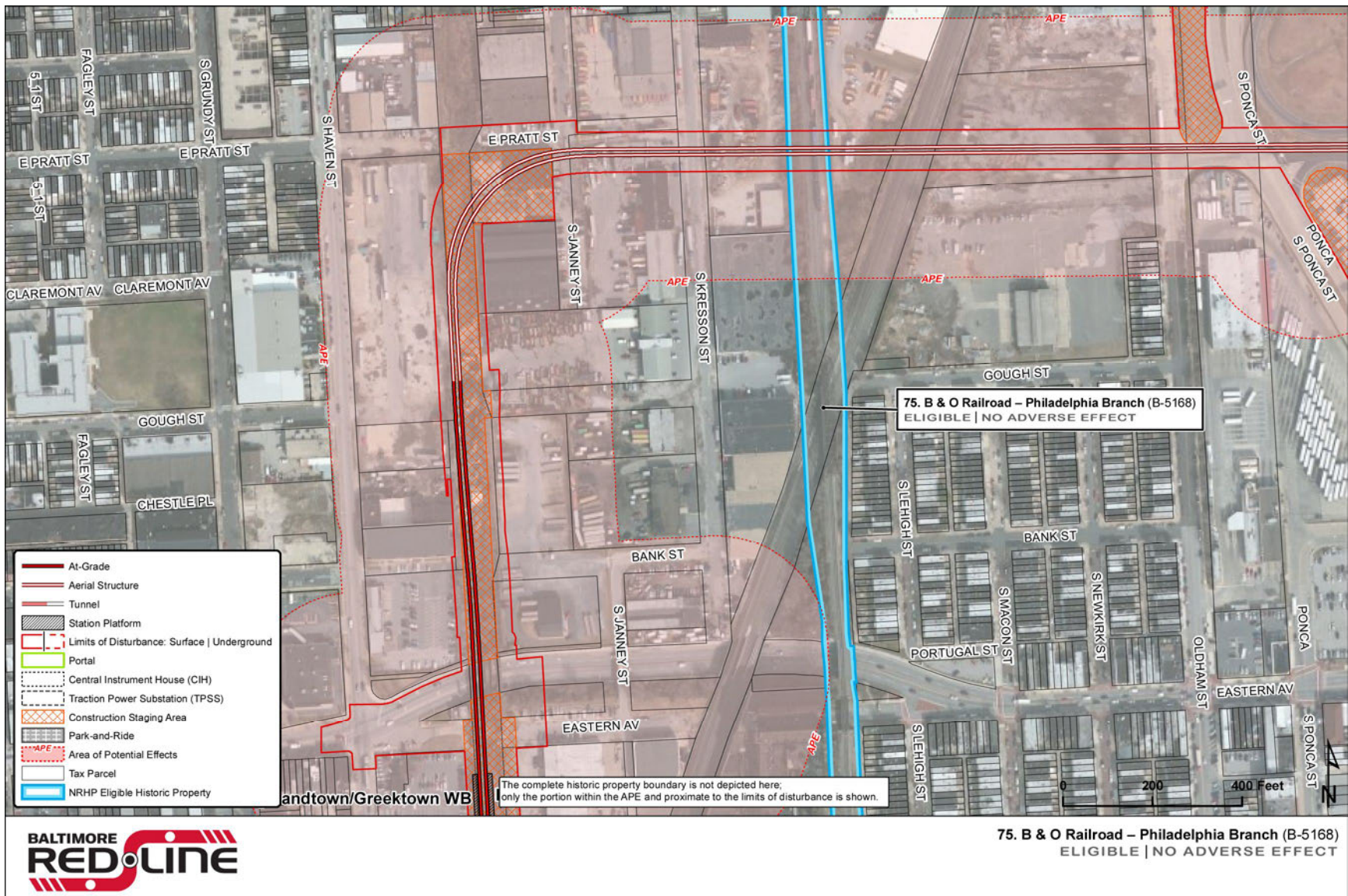


Figure 409. Proposed project in vicinity of B&O Railroad – Philadelphia Branch



Figure 410. View east from near the proposed alignment's aerial structure (at East Pratt and South Kresson streets) toward the Baltimore & Ohio Railroad-Philadelphia Branch (beyond the trees)



Figure 411. View southwest from East Lombard Street toward the proposed alignment's aerial structure (along East Pratt Street) over the Baltimore & Ohio Railroad-Philadelphia Branch (railroad runs on the metal truss bridge in the background)

76. Highlandtown Pumping Station

4118 Eastern Avenue

MIHP No. B-5171

The Highlandtown Pumping Station is a one-story, brick municipal building with cast concrete ornamentation and an attached open terrace on its west side. Although modest in scale, it was built at a time when public buildings, even sewage pumping stations, were a symbol of public pride and adorned accordingly. The Highlandtown Pumping Station was built in 1930 and incorporated then-state-of-the-art engineering. The building was determined eligible for listing in the NRHP under Criterion A for its association with Baltimore's infrastructure developments and under Criterion C for its engineering applications for modern sanitary facilities and its classically inspired architectural details applied to a utilitarian public building.

Near the Highlandtown Pumping Station, Red Line Project implementation would include construction of the alignment immediately east of the property, construction of the Highlandtown/Greektown Station to the property's southeast, and minor improvements to surrounding existing streets and sidewalks, including along Eastern Avenue and South Haven Street. The proposed alignment, including dual tracks and the catenary system's support poles, wires, and some lighting, would be located in the former Union Railroad right-of-way just east of the Highlandtown Pumping Station. The proposed split-platform Highlandtown/Greektown Station, consisting of two platforms 190 feet long and 15 feet wide with a partial canopy, would also be located in the rail right-of-way approximately 125 feet south of the property. The project's LOD extends west from the alignment along Eastern Avenue and encompasses the Highlandtown Pumping Station; however, no project activity is proposed within the property's NRHP boundary with the possible exception of minor landscape improvements. Project activity along Eastern Avenue and South Haven Street would be limited to existing street improvements, including minor roadway and sidewalk upgrades that would be conducted within existing rights-of-way. Temporary project vehicle storage may occur just south of the property's NRHP boundary.

Potential physical impacts to the Highlandtown Pumping Station would not be adverse; no project activity is proposed within the property's NRHP boundary, with the possible exception of limited landscape improvements. Minor landscape improvements would not alter any physical, character-defining features of the property. Therefore, no adverse effects to the property's integrity of location, design, materials, and workmanship would occur.

The Highlandtown Pumping Station does not retain integrity of setting due to non-historic construction in its vicinity; furthermore, the property's setting is not a historically significant or character-defining feature. Although proposed project elements, including the alignment to the property's east, the station to its southeast, and minor street and sidewalk improvements to its west, would be visible from the property, they would represent a minor change to the property's altered setting. Potential project construction vehicle storage to the property's south would be temporary and would not affect the setting. Likewise, any potential landscape improvements within the property's NRHP boundary would not adversely affect the property's setting, which is not a character-defining feature. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property's

setting would be impacted. Because no historically significant views would be obscured, no adverse visual effects to the property were identified. Therefore, project implementation would have no adverse effect to the Highlandtown Pumping Station's integrity of setting.

Furthermore, no project activity would alter the property's feeling as an early twentieth-century municipal building incorporating classically inspired architectural details or its association with the development of Baltimore's public utilities system. Therefore, project implementation would have no effect to the property's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no adverse effect** to the Highlandtown Pumping Station.

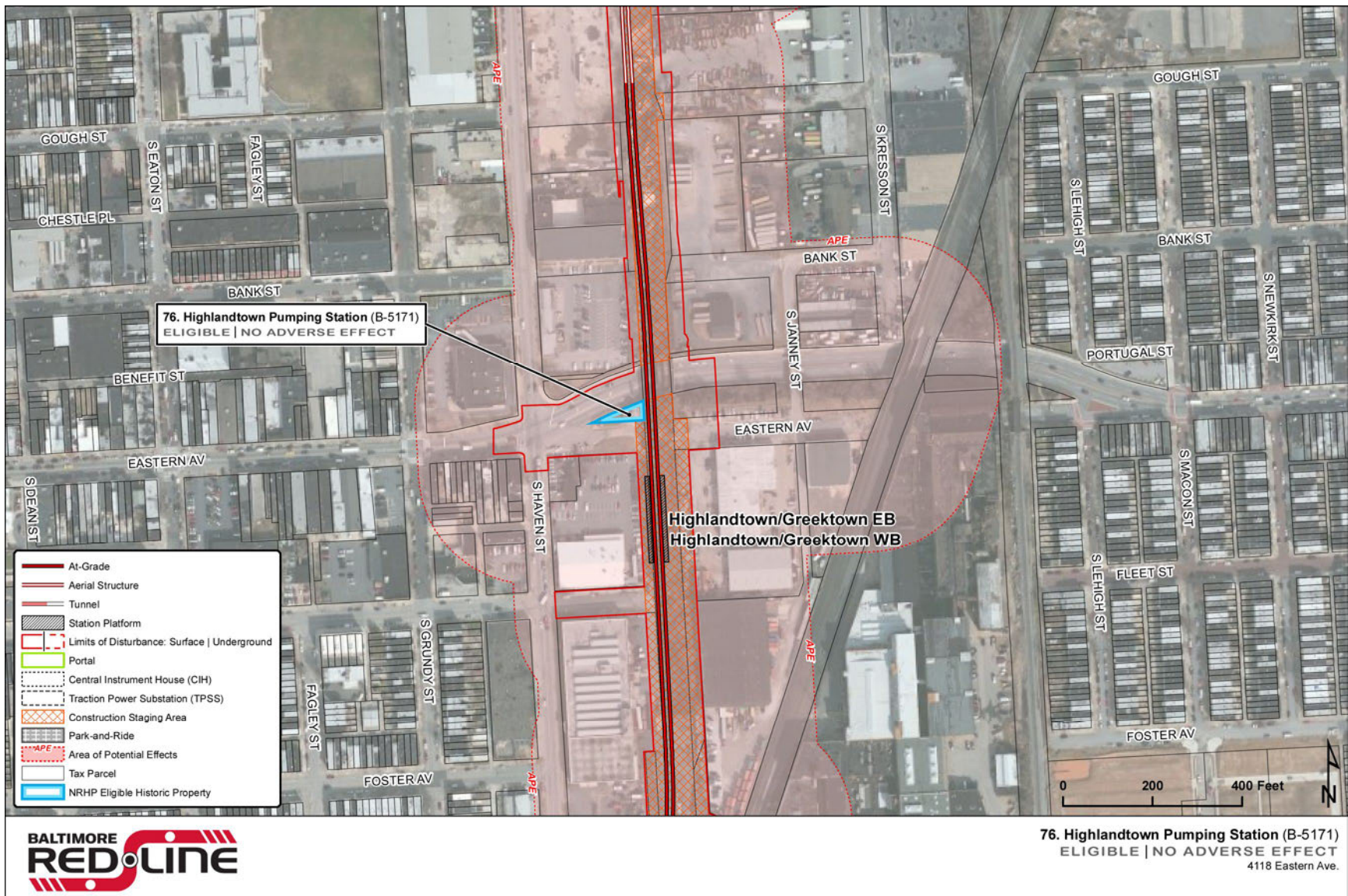


Figure 412. Proposed project in vicinity of Highlandtown Pumping Station



Figure 413. View east toward the proposed alignment on Union Railroad bridge over Eastern Avenue Underpass and to the Highlandtown Pumping Station (see red arrow)



Figure 414. View northwest toward the Highlandtown Pumping Station from the proposed at-grade alignment on Union Railroad



Figure 415. View southeast toward the proposed at-grade alignment on Union Railroad from the Highlandtown Pumping Station

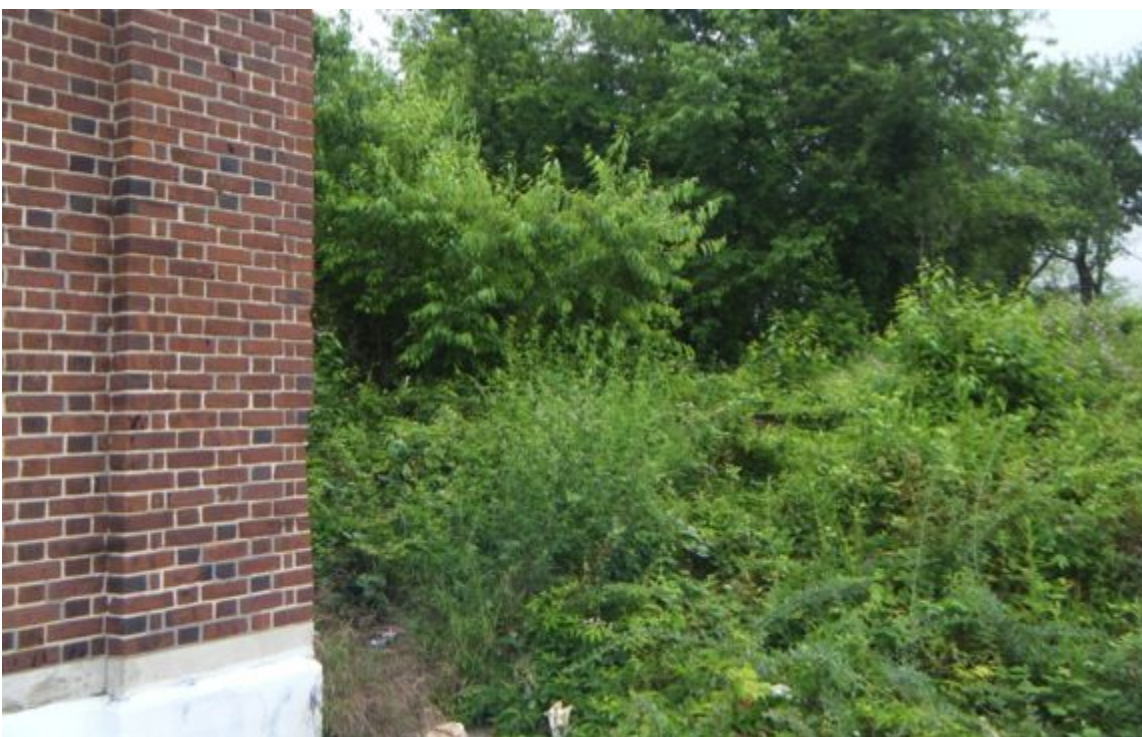


Figure 416. View northeast toward the proposed at-grade alignment on Union Railroad from the Highlandtown Pumping Station

77. Greektown Historic District

Generally bounded by Gough Street and Eastern Avenue on the north, Oldham and South Quail streets on the east, Hudson Street and Foster Avenue on the south, and B & O Railroad on the west

MIHP No. B-1368

The Greektown Historic District is a historically Greek ethnic neighborhood predominately consisting of residential row houses constructed from the 1910s to 1950s, as well as mixed use residential and commercial buildings along Eastern Avenue and two churches. The district's houses are typical and modest working-class Baltimore row houses; most are two stories, clad in Formstone, and have no identifiable style. The mixed-use buildings along Eastern Avenue comprise the neighborhood's central commercial area and contain Greek restaurants and Greek-focused businesses. Greek immigrants began to settle in the Baltimore area in the late nineteenth century, eventually moving to the present-day Greektown area by the 1920s and establishing a distinct neighborhood culture incorporating Greek traditions. The Greek population rose first in the 1930s and again in the 1960s; residents of Greek descent continue to live in Greektown. The Greektown Historic District was determined eligible for listing in the NRHP under Criterion A for its association with immigrant and settlement patterns in Baltimore and its embodiment of a cohesive and distinct Greek-American ethnic neighborhood.

Near the Greektown Historic District, Red Line Project implementation would include the installation of the alignment along Union Railroad west of the district boundaries and the installation of elevated alignment crossing over parcels north of the district boundaries. The at-grade alignment, consisting of dual tracks and the overhead catenary system, including support poles, wires, and some lighting, would be installed along the former Union Railroad right-of-way approximately 840 feet west of the district's west NRHP boundary. The proposed split-platform Highlandtown/Greektown Station, which would consist of two platforms approximately 190 feet long and 15 feet wide with a partial canopy, would be located within the former Union Railroad right-of-way, just south of Eastern Avenue and approximately 830 feet west of the district's west NRHP boundary. The alignment would ascend from grade to elevated track near Gough Street and turn east approximately 400 feet northeast of the South Haven Street/Gough Street intersection. The elevated alignment would be located and cross over parcels approximately 200 feet north of the district's northernmost NRHP boundary.

No physical impacts to the Greektown Historic District would occur; no project activity is proposed within the district's NRHP boundary. Therefore, no effects to the district's integrity of location, design, materials, and workmanship would occur.

The Greektown Historic District's integrity of setting is greatly diminished by the construction of I-895 and I-95 to the east and south of the district, although the district's streetscapes are character-defining features. To the district's north, the proposed elevated project components would be minimally visible from some properties along the district's northern boundaries but would not adversely affect the district's already altered visual setting. Further, an intervening industrial yard and building and vegetation would partially obscure some views to and from the district. To the district's west, numerous intervening buildings obstruct views to and from the district and the at-grade alignment and the proposed station. No historically significant views to

or from the district would be obscured by project implementation, and no character-defining features of the district's setting would be impacted. Because no historically significant views would be obscured, no visual effects to the district were identified. Therefore, project implementation would have no adverse effect to the Greektown Historic District's integrity of setting.

Furthermore, no project activity would alter the district's feeling as a cohesive and distinct Greek-American ethnic neighborhood, or its association with immigrant and settlement patterns in Baltimore. Therefore, project implementation would have no effect to the district's integrity of feeling or association.

Based on this evaluation, the Red Line Project would have **no adverse effect** to the Greektown Historic District.

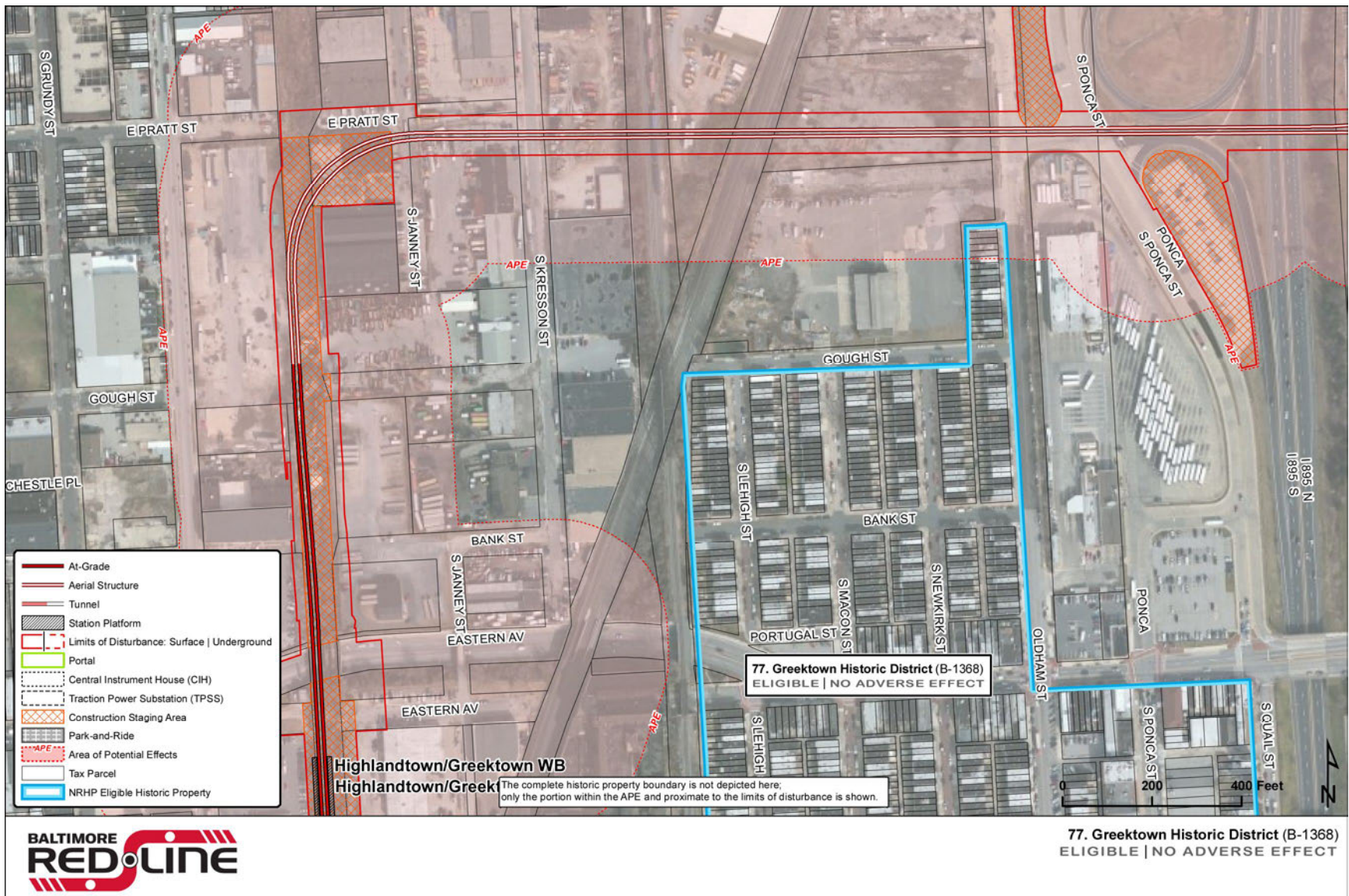


Figure 417. Proposed project in vicinity of Greektown Historic District



Figure 418. View southwest toward the Greektown Historic District (along Oldham Street) from the vicinity of the proposed alignment's aerial structure along East Pratt Street

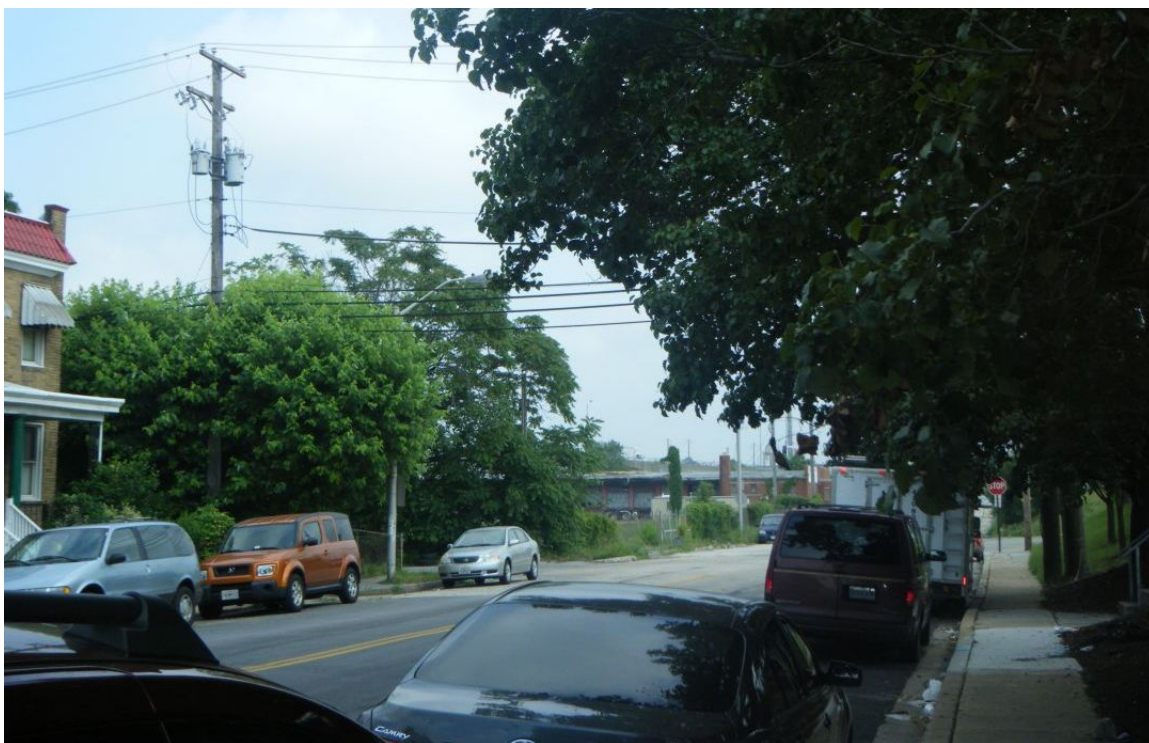


Figure 419. View northwest from within the Greektown Historic District (along Oldham Street) toward the proposed alignment's aerial structure along East Pratt Street



Figure 420. View north from the edge of the Greentown Historic District (along Oldham Street) toward the proposed alignment's aerial structure along East Pratt Street

78. Johns Hopkins Bayview Hospital Campus

4940-5550 Eastern Avenue

MIHP No. B-5176

Johns Hopkins Bayview Hospital Campus is an active medical complex that contains historic buildings as well as recently constructed facilities. The site was first associated with public healthcare in the 1770s. Within the historic district's period of significance, three distinct eras of historic architecture remain: portions of an 1860s asylum; 1930s Art Deco buildings; and 1950s buildings. The Johns Hopkins Bayview Hospital Campus was determined eligible for listing as a historic district in the NRHP in 2010. It is eligible under Criterion A for its association with public healthcare and associated improvements over 250 years in Baltimore. It is also eligible under Criterion C for its Art Deco architecture, which embodies character-defining features of the style and is a unique example as applied to a public institution.

At the Johns Hopkins Bayview Hospital Campus, the Red Line alignment would be transitioning from an elevated alignment to at grade; the touch-down occurs just within the western boundary of the historic district within a non-historic parking lot. The at-grade alignment within the historic district would consist of trackwork, overhead catenary lines, and support poles, some with lighting; this work would occur on existing roadways within the historic district, including Alpha Commons Drive and Bayview Boulevard, with the exception of a small area just south of the Bayview Boulevard/Lombard Street intersection that is not proximate to any contributing resources. Additional project components proposed within the Johns Hopkins Bayview Hospital Campus consist of the construction of the above-ground Bayview Campus Station that would be built within the historic district. The station would be located on a parking lot just north of Alpha Commons Drive and west of Bayview Boulevard. The station would consist of a platform that is approximately 190 feet long and 15 feet wide with a partial canopy. A traction power substation would be located within the western edge of the district along an existing roadway adjacent to a parking lot; visual screening measures may be implemented if appropriate.

No physical impacts to contributing historic resources within the Johns Hopkins Bayview Hospital Campus would occur, although project activity is proposed within the property's NRHP boundary. Therefore, no adverse effects to the property's integrity of location, design, materials, or workmanship would occur.

The integrity of setting within the district, as well as in surrounding areas, has been diminished over time by demolitions; new building construction; I-895 and other roadway construction; and alterations to existing buildings. The elevated alignment through an industrial yard and over I-895 would not adversely affect the historic district's integrity of setting; the alignment returns to grade just within the district. Locating a station in a parking lot and a traction power substation along an existing street on the Johns Hopkins Bayview Hospital Campus would not adversely affect the setting. Likewise, adding trackwork, poles, and catenary lines on the campus would have no adverse effect on the setting. These project facilities represent a minor alteration to the large property's already altered visual setting. No historically significant views to or from the property would be obscured by project implementation, and no character-defining features of the property's setting would be impacted. Because no historically significant views would be

obscured, no visual effects to the property were identified. Therefore, project implementation would have no adverse effect to the property's integrity of setting.

The Johns Hopkins Bayview Hospital Campus retains integrity of feeling and association. Character-defining features that convey the complex's expression of its aesthetic and period in time, as well as its association with Baltimore's medical history, are present and would not be affected by the Red Line Project. The campus has evolved over time and contains a circulation system and extensive parking areas that would essentially conceal the proposed alignment nearly completely within the historic district. The project would have no adverse effect on the property's integrity of feeling and association.

Based on this evaluation, the Red Line Project would have **no adverse effect** to the Johns Hopkins Bayview Hospital Campus.

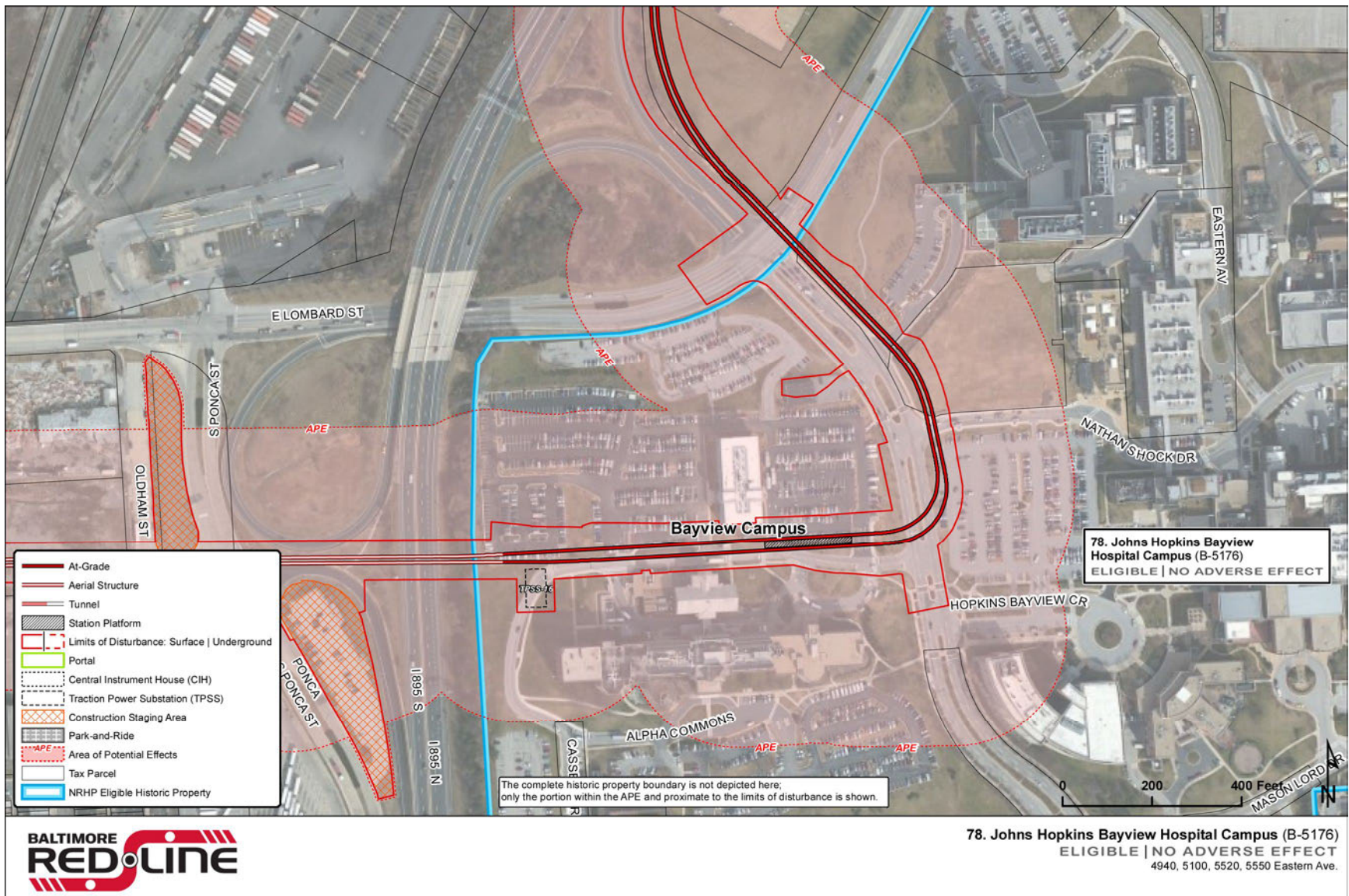


Figure 421. Proposed project in vicinity of the Johns Hopkins Bayview Hospital Campus



Figure 422. View east along Alpha Commons Drive and proposed alignment toward Bayview Boulevard within the Johns Hopkins Bayview Hospital Campus; proposed traction power substation would be located in foreground



Figure 423. View west along Alpha Commons Drive toward the proposed at-grade alignment (along the road and to north) from the Alpha Center on the Johns Hopkins Bayview Hospital Campus



Figure 424. View east along Alpha Commons Drive and the proposed at-grade alignment to the proposed Bayview Campus Station (both along the road and to its north) from the Alpha Center on the Johns Hopkins Bayview Hospital Campus



Figure 425. View west along proposed at-grade alignment on Alpha Commons Drive to proposed Bayview Campus Station (both along the road and to its north) within the Johns Hopkins Bayview Hospital Campus; Alpha Center and Mason F. Lord Building to south



Figure 426. View east along Alpha Commons Drive and proposed at-grade alignment and Bayview Campus Station (both along the road and to its north within the Johns Hopkins Bayview Hospital Campus; Mason F. Lord Building to south



Figure 427. View southwest along Bayview Boulevard and proposed at-grade alignment (to roadway's west) and to Mason F. Lord Building within Johns Hopkins Bayview Hospital Campus

Works Consulted

Maryland Historical Trust. Maryland Department of Planning. *Standards and Guidelines for Architectural and Historical Investigations in Maryland*. Crownsville, 2000.

Maryland Historical Trust. Determination of Eligibility Forms for resources within the Area of Potential Effects. On file at the Maryland Historical Trust Library, Crownsville, Maryland.




Section 106 of the National Historic Preservation Act, 36 C.F.R. § 800 (2012).




U.S. Department of the Interior. National Park Service. *National Register Bulletin: How to Apply the National Register Criteria for Evaluation*, by the staff of the National Register of Historic Places, finalized by Patrick W. Andrus, edited by Rebecca H. Shrimpton. Washington, DC, 1992.




U.S. Department of the Interior. National Park Service. *National Register Bulletin: Defining Boundaries for National Register*. *Defining Boundaries for National Register Properties*, by Donna J. Seifert including Barbara Savage, Beth L. Savage, and John H. Sprinkle, Jr., Washington, DC, 1995, revised 1997.




U.S. Department of the Interior. National Park Service. National Register of Historic Places. National Register of Historic Places forms for various historic properties within the Area of Potential Effects. On file at the National Register of Historic Places, Washington, D.C.




Appendix: All Historic Properties Located Within the Area of Potential Effects



MAP NO.	PHOTOGRAPH	NAME/LOCATION (IN BALTIMORE CITY EXCEPT WHERE OTHERWISE NOTED)	MHT NO.	DESCRIPTION	BUILD DATE	NRHP STATUS	NRHP CRITERIA	EFFECT
1		<i>Franklinton Road over Dead Run Bridge</i> (SHA # B0096) Franklinton Rd. over Dead Run located east of Security Blvd., Woodlawn, Baltimore County	BA-2853	75 feet long double span filled concrete arch bridge	1928	Eligible	C	No Effect
2		<i>Franklinton Historic District</i> 5100-5201 N. Franklinton Rd., 1707-1809 N. Forest Park Ave., 5100 Hamilton Ave., 5100 Fredwall Ave.	B-1316	A planned resort community centered around an oval wooded park, with radiating wedge-shaped lots with a mill complex, an inn, houses, and commercial buildings	ca. 1832 to mid-20 th century	Listed	A, C	No Effect
3		<i>Gwynns Falls Park/Leakin Park</i> West Baltimore, Gwynns Falls Valley	B-4610	Large, naturally landscaped park designed by Olmsted Brothers Landscape architects	Circa 1901 to 1948	Eligible	A, B, C	No Effect




MAP NO.	PHOTOGRAPH	NAME/LOCATION (IN BALTIMORE CITY EXCEPT WHERE OTHERWISE NOTED)	MHT NO.	DESCRIPTION	BUILD DATE	NRHP STATUS	NRHP CRITERIA	EFFECT
4		<i>St. William of York Catholic Church and School</i> 600 Cooks Ln.	B-5100	2.5-story Gothic Revival church faced with rusticated granite and carved limestone, and with a 1-story front entrance section; a 2-story rusticated stone rectory in a collegiate Gothic/Tudor style attached to the church; a Georgian Colonial Revival brick convent and a school adjacent to the church; there is a parking lot at the rear of the property	1) church 1914; 2) school and smaller wing of the school ca. late 1940s; 3) rest of school 1951	Eligible	C, Criteria Consideration A	No Adverse Effect
5		<i>Ten Hills Historic District</i> Roughly bounded by Uplands Park, Edmondson Ave., and Westgate Rd.	B-5124	A planned suburban upper middle class residential neighborhood that incorporated the natural setting; many architect designed detached houses; architectural styles include Colonial Revival, Tudor Revival, Craftsman, Spanish/Mission Revival, French Normandy, Italian Renaissance Revival, and Ranch	ca. 1909 to 1960	Eligible	A, C	No Adverse Effect
6		<i>Hunting Ridge Historic District</i> Bounded by Edmondson Ave., Nottingham Rd., Leakin Park, Winans Wy., and Glen Allen Dr.	B-5125	A planned suburban middle class residential neighborhood that incorporated the natural setting; architectural styles include Shingle, Colonial Revival, Dutch Colonial, Cape Cod, Tudor Revival, Craftsman, and Ranch	1923 to mid-20 th century	Eligible (the Hunting Ridge Presbyterian Church is within this district and also individually eligible)	A, C	No Adverse Effect




MAP NO.	PHOTOGRAPH	NAME/LOCATION (IN BALTIMORE CITY EXCEPT WHERE OTHERWISE NOTED)	MHT NO.	DESCRIPTION	BUILD DATE	NRHP STATUS	NRHP CRITERIA	EFFECT
7		<i>Hunting Ridge Presbyterian Church</i> 4640 Edmondson Ave.	B-5106	A 2.5-story brick and stucco Tudor Revival church building and a 2.5-story brick church school building; on a 1.66-acre parcel with a large paved parking lot	church 1930; school 1951-52	Eligible <i>(also located within the Hunting Ridge District)</i>	C, Criteria Consideration A	No Adverse Effect
8		<i>Rognel Heights-Ten Hills School (Public School #232, Thomas Jefferson Elementary School)</i> 605 Dryden Dr.	B-4614	Long 1-story brick original section designed in the Renaissance Revival style with an International Style addition	1925	Eligible	A, C	No Effect
9		<i>St. Bartholomew's Episcopal Church</i> 4711 Edmondson Ave.	B-5105	2.5-story Gothic Revival stone church with a 3-story bell tower and a detached 2-story Tudor Revival brick parish hall; located on a 2.5-acre wooded property	church 1931-32; parish hall earlier	Eligible	C, Criteria Consideration A	No Adverse Effect




MAP NO.	PHOTOGRAPH	NAME/LOCATION (IN BALTIMORE CITY EXCEPT WHERE OTHERWISE NOTED)	MHT NO.	DESCRIPTION	BUILD DATE	NRHP STATUS	NRHP CRITERIA	EFFECT
10		<i>Baltimore Fire Department Co. No. 53</i> 608 Swann Ave.	B-5126	2-story wood shingle and stucco Colonial Revival/Shingle fire station with a 4-story brick hose tower	1921-22	Eligible	A, C	No Effect
11		<i>Edmondson Village Shopping Center</i> 4404 Edmondson Ave.	B-4223	Early brick Colonial Revival shopping center located on an 11-acre parcel with paved parking areas in front and behind; originally a long row of approximately 17 brick building sections ranging from 1.5-story to 2.5-story; the west most section was demolished after a 2008 fire and rebuilt in 2010 in compatible style and materials	1946-47	Eligible	A, C	No Adverse Effect
12		<i>Roguel Heights Historic District</i> Walnut Ave. and cross streets north of Edmondson Village Shopping Center	B-5108	A wooded, grid-pattern neighborhood of detached single homes; architectural styles include Queen Anne, Shingle, American Foursquare, Craftsman, and hybrids of these styles	late 19 th to early 20 th centuries	Eligible	A, C	No Effect




MAP NO.	PHOTOGRAPH	NAME/LOCATION (IN BALTIMORE CITY EXCEPT WHERE OTHERWISE NOTED)	MHT NO.	DESCRIPTION	BUILD DATE	NRHP STATUS	NRHP CRITERIA	EFFECT
13		<i>Enoch Pratt Free Library, Edmondson Avenue Branch</i> 4330 Edmondson Ave.	B-1384	Brick Colonial Revival library with a 2-story main corner section and two 1-story side wings	1952	Eligible	A, C	No Adverse Effect
14		<i>New Cathedral Cemetery</i> 4300 Old Frederick Rd.	B-5110	A large Catholic garden cemetery on a rolling 122-acre parcel, with intersecting curvilinear paved roads, a hilly topography and high elevation; a small lake near the center and a tall iron fence surrounds the property; graves with a variety of stone markers, many of which are elaborate monuments with carved statues, columns, crosses, obelisks, and other details; crypts and two old frame buildings, a caretaker's house and a small chapel	ca. 1870-71	Eligible	C, Criteria Consideration D	No Adverse Effect
15		<i>Edmondson Village Historic District</i> North side of Edmondson Ave. between alley west of Woodington Rd. and Hilton St., and roughly bounded on north by Gelston Dr.	B-5109	Mostly early 2- and 2.5-story row houses (with some 3-story examples) in a variety of architectural styles including Italianate, Colonial Revival, Tudor Revival, Craftsman, Spanish Revival, and hybrids of these styles; also some commercial, religious, and educational buildings	ca. 1911 to 1938	Eligible	A, C	No Adverse Effect




MAP NO.	PHOTOGRAPH	NAME/LOCATION (IN BALTIMORE CITY EXCEPT WHERE OTHERWISE NOTED)	MHT NO.	DESCRIPTION	BUILD DATE	NRHP STATUS	NRHP CRITERIA	EFFECT
16		<i>Allendale-West Mulberry Historic District</i> South side of Edmondson Ave., roughly bounded by Wildwood Pkwy., New Cathedral Cemetery, W. Mulberry St., Gwynn Ave., N. Monastery Ave., N. Culver St., and Hilton St.	B-5111	Early middle-class suburban 2-story brick duplexes and row houses (with some 2.5- and 3-story examples); architectural styles include Italianate, Colonial Revival, Tudor Revival, Craftsman, Spanish Revival, and hybrids of these styles; a few commercial and institutional properties	1910s to mid-1930s	Eligible	A, C	No Adverse Effect
17		<i>Keelty Daylight Rowhouse Historic District at Gwynns Falls</i> Two sections located on the west and east sides of Gwynns Falls Park: (1) the west section is bordered by Normandy Ave./Lyndhurst St., Gelston Dr., N. Hilton St., Mulberry St., Edgewood St., W. Lexington St., N. Grantley St., W. Saratoga St., and Allendale St. and (2) the east section is bordered by Gwynns Falls Trail, Ellicott Dwy., Braddish Ave., W. Lafayette Ave., Poplar Grove St., and Edmondson Ave.	B-1378	A series of speculative two-, two-and-one-half- and three-story, three-bay, brick and stone "daylighter" row house developments with front porches, and Italianate, Colonial Revival, Tudor Revival and Craftsman details; created by developer James Keelty	1910s to 1930s	Eligible	A, C	No Adverse Effect




MAP NO.	PHOTOGRAPH	NAME/LOCATION (IN BALTIMORE CITY EXCEPT WHERE OTHERWISE NOTED)	MHT NO.	DESCRIPTION	BUILD DATE	NRHP STATUS	NRHP CRITERIA	EFFECT
18		<i>Western Maryland Railroad-Tidewater Extension (CSX Tracks)</i>	B-1377	A relatively short spur of track along the Gwynn Falls	ca. 1904	Eligible	A	No Effect
19		<i>Greater Rosemont Historic District</i> North side of Franklin Ave., roughly bounded by N. Rosedale St., Ellicott Driveway, Ashburton St., Rayner Ave., Whitmore Ave., Riggs Ave., N. Warwick Ave., W. Lafayette Ave., Wheeler Ave., Winchester St., and Penn Central RR tracks	B-5112	Early middle-class suburban 2-story brick row houses and duplexes (with some 3-story examples); architectural styles include Italianate and Art Deco, and others display characteristics of Colonial Revival, Spanish Revival, and Craftsman; some commercial, industrial, and institutional buildings	ca. 1890 to 1950s	Eligible	A, C	No Adverse Effect
20		<i>Edmondson Avenue Historic District</i> (overlaps with the Greater Rosemont District above)	B-5187	2- and 2 1/2-story rowhouses and duplexes; mostly "daylighters," though older houses area also present; a variety of architectural styles include Italianate, Colonial Revival, Tudor Revival, Craftsman, Spanish, Art Deco, and hybrids of these styles; most are of brick construction, with a few stucco examples; includes a garden apartment complex, churches, and a school	early to mid-20 th century	Listed 12/27/2010	A, C	No Adverse Effect



MAP NO.	PHOTOGRAPH	NAME/LOCATION (IN BALTIMORE CITY EXCEPT WHERE OTHERWISE NOTED)	MHT NO.	DESCRIPTION	BUILD DATE	NRHP STATUS	NRHP CRITERIA	EFFECT
21		<i>Baltimore & Potomac Railroad (Philadelphia, Baltimore & Washington Railroad; elsewhere: Philadelphia Wilmington & Baltimore Railroad)</i> Between the Baltimore City/Baltimore County line (in the community of Violetville) at the southwest to Baltimore's Pennsylvania Station at the northeast (excluding the station itself)	B-5164	Railroad alignment with generally double tracks or four tracks; alignment also includes the Baltimore & Potomac Tunnel, catenary lines, four railroad bridges, the two-story Gwynn Junction Tower, the West Baltimore MARC Station, and ancillary buildings at the north end of the segment	Est. 1872; tunnel (1872); most other structures and buildings from the early part of the 20 th century	Eligible	A, C	No Adverse Effect
22		<i>American Ice Company</i> 2100 W. Franklin St. (northeast corner of W. Franklin St. and MARC Penn Line tracks)	B-1040	1- to 2-story brick industrial building in three sections that formerly housed the boiler room, engine room, and tank house; the ice house and ancillary section of the building, and a free standing office, are now demolished; concrete pads remain of former buildings on the rest of the property	1896; with 1950s and 1990s rear additions	Eligible	A, C	No Effect
23		<i>Monroe-Riggs Historic District</i> Roughly bounded by Penn Central tracks, Franklin St., alley west of Fulton Ave., and Riggs St. (adjoins Old West Baltimore Historic District)	B-5118	Middle- and working-class 2- and 3-story brick row houses; some covered with Formstone or stucco; architectural styles include Italianate and Richardsonian Romanesque; also a commercial area including a Spanish Revival style gas station	residential ca. 1890 to 1915; comm. properties ca. 1910 to 1930	Eligible	A, C	No Adverse Effect




MAP NO.	PHOTOGRAPH	NAME/LOCATION (IN BALTIMORE CITY EXCEPT WHERE OTHERWISE NOTED)	MHT NO.	DESCRIPTION	BUILD DATE	NRHP STATUS	NRHP CRITERIA	EFFECT
24		<i>Bon Secours Historic District</i> Roughly bounded by W. Mulberry St., N. Monroe St., W. Baltimore St., N. Calverton Rd., N. Warwick Ave., W. Lexington St., and N. Bentalou St.	B-5117	Typically 2-story middle- and working-class brick row houses; architectural styles include Italianate, Richardsonian Romanesque, and others have Colonial Revival, Craftsman, and Spanish Revival details; Bon Secours Hospital located within the district	ca. 1890s to 1940	Eligible	A, C	No Adverse Effect
25		<i>Franklin Square Historic District</i> Bounded by Mulberry, N. Carey, W. Baltimore, and Monroe Sts.	B-3610	3-story brick row houses in an urban residential district with the 2.5-acre Franklin Square; also several large stone churches in the Franklin Square area	ca. mid- to late 19 th century	Listed	A, C	No Adverse Effect
26		<i>Harlem Park Historic District</i> W. Lanvale St. to the north, N. Fremont Ave. to the east, W. Franklin St. to the south, and N. Monroe St. to the west (except for portion between Lanvale, N. Calhoun St., Edmondson Ave., and N. Gilmore St.)	B-1320	Row house neighborhood centered around a large park; most buildings feature traditional flat brick façades with ornate cornices, marble steps, and water tables; also a few stone façades as well as stone churches	mid- to late 19 th century	Eligible	A, C	No Adverse Effect




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27		<i>Old West Baltimore Historic District</i> Roughly bounded by North Ave., Madison Ave., Dolphin St., Hoffman St., Fremont Ave., Franklin St., and Fulton Ave.	B-1373	Urban residential district of approximately 175 city blocks with primarily brick row houses; also mansions and alley houses, churches, public buildings (primarily schools), commercial buildings, and landscaped squares; an African-American neighborhood beginning in the 1890s	ca. 1840 to 1920	Listed	A, C	No Adverse Effect
28		<i>Sarah Ann Row Houses</i> 1102-24 Sarah Ann St.	B-2427	A group of twelve 2-story brick alley row houses	1870s	Eligible	A	No Effect
29		<i>Hollins-Roundhouse Historic District</i> W. Baltimore and Schroeder Sts, S on Schroeder to Lombard; W on Lombard to Carey, S to Pratt, E on Pratt to Hayes	B-5144	Residential institutional, and commercial district primarily associated with immigrant employees of the Baltimore & Ohio Railroad; residences include Greek Revival and Italianate townhouses, and “alley house” and “half-house” types	1835-1945	Listed	A, C	No Effect




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30		<i>Fayette Street Methodist Episcopal Church</i> 745-51 W. Fayette St.	B-2702	3-story, five bay Romanesque Revival style brick and stone church	ca. 1865-70 facade on a ca. 1850-55 building	Eligible	A,C	No Adverse Effect
31		<i>Poppleton Fire Station (Engine House No. 38)</i> 756-60 W. Baltimore St.	B-3693	2-story brick and limestone Tudor Revival firehouse with a central, Tudor archway flanked by octagonal towers and crowned with crenellation	1910	Listed	A, C	Adverse Effect
32		<i>St. Paul's Cemetery (Old St. Paul's Cemetery)</i> W. Redwood St. to the north, W. Lombard St. to the south, S. Martin Luther King, Jr. Blvd. to the west, and the University of Maryland campus to the east	B-3686	2.4 acre early detached cemetery surrounded by rubble stone walls; graves markers range from simple and plain stones standing erect or flat on the ground to columned monuments and tablets	laid out in 1799	Listed	A, C and Criteria Consideration D	No Effect




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33		<i>Barre Circle Historic District</i> Roughly bounded by Boyd, Scott and Ramsey Sts., and S. Martin Luther King, Jr. Blvd.	B-3701	Urban working class residential district of approximately 200 modest 2- to 3-story brick row houses	ca. mid-19 th to early 20 th century	Listed	A, C	No Effect
34		<i>University of Maryland-Fremont Building</i> 737 W. Lombard St.	B-3594	Six-story, brick commercial loft building	1906	Eligible	C	No Effect
35		<i>Gandy Belting Company Building</i> 726-34 W. Pratt St.	B-4092	4- to 5-story brick bearing-wall loft building; low pedimented parapets with inset stone plaques with the year of construction located at the facade	seven sections built in 1888, 1890, 1908, 1908-11, 1911	Listed	A, C	No Effect



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36		<i>Penn Street Sub-Station (Baltimore Gas and Electric Substation)</i> 700-26 W. Pratt St.	B-1053	Only facade remains; previously determined eligible	n.d.	Eligible	A,C	No Effect
37		<i>University of Maryland Medical School – Hospital District</i> Buildings near intersections of S. Greene with W. Redwood, and W. Lombard Sts.	B-5128	A collection of seven of the oldest extant university buildings of varying styles and periods: 1) Davidge Hall, 2) UMD Law School/University College/East Hall, 3) Gray Laboratory, 4) Dental-Pharmaceutical Building/Medical-Technical Building/UMD Department of Preventative Medicine/National Museum of Dentistry, 5) UMD Law School/UMD School of Social Work, 6) University of Maryland Bressler Memorial Laboratory, and 7) University Hospital; three of the buildings from the original evaluation, have since been demolished: 1) Lombard Building, 2) 111 S. Greene Street, and 3) Student Union building	ranging from 1811-12 to 1933-34	Eligible (<i>Dental and Pharm. Building, Davidge Hall, UMD Law School, and Heiser, Rosenfeld, and Strauss Buildings are within this district and also individually listed or eligible</i>)	A	No Effect




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38		<i>UMD-Bressler Memorial Laboratory Bldg.</i> 29 S. Greene St.	B-3589	Tiered 6-story oblong Art Deco medical school building clad in common-bond brick with limestone and concrete trim	1939	Eligible (also within UMD Medical School Hospital District)	A, C	No Effect
39		<i>Dental and Pharmaceutical Building (Medical-Technical Building, UMD Dept. of Preventative Medicine)</i> 31 S. Greene St.	B-2327	Brick and gray limestone Renaissance Revival medical school building with three full stories set atop an English basement level	1904	Eligible (also within UMD Medical School Hospital District)	C	No Effect
40		<i>Davidge Hall, University of Maryland (College of Medicine of Maryland)</i> 522 W. Lombard St.	B-41	Pantheon-style domed brick and wood Greek Revival building with two circular amphitheaters; oldest building in the United States used continuously for medical education	1811-13	Listed (also NHL and within UMD Medical School Hospital District)	A, C	No Effect




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41		<i>University of Maryland Law School/University College/East Hall</i> 520 W. Lombard St.	B-2326	The older section, located at the rear, is a 1-story brick building, and the newer section is a 2.5-story brick Classical Revival building with stone accents and wooden trim, set on an English basement	original section 1884; newer section 1907	Eligible (also within UMD Medical School Hospital District)	A	No Effect
42		<i>Gray Laboratory</i> 520 W. Lombard St.	B-3583	4-story red brick building with a shallow-pitched shed roof and two perpendicular wings forming an L-shaped footprint; only the south and west elevations are visible, the rest backing up to adjacent buildings	1894	Eligible (also within UMD Medical School Hospital District)	A, C	No Effect
43		<i>Loft Historic District South</i> Both sides of the 500 block of W. Pratt St. bounded by S. Greene and S. Paca Sts., near the University of Maryland campus	B-4094	Seven large brick manufacturing buildings stylistically representative of Romanesque, Victorian, and Neo-Classical early modern industrial architecture; most are from 5- to 9-story but two are 2-story	1890 to 1905	Listed (Sonneborn Building is within this district and individually listed)	A, C	No Effect




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44		<i>Sonneborn Building (Paca-Pratt Building)</i> 110 S. Paca St.	B-2330	9-story “fireproof” reinforced concrete Neoclassical Revival loft industrial building faced in buff-colored brick, with a coursed ashlar foundation and limestone trim	1905	Listed <i>(also within Loft HD South)</i>	A, C	No Effect
45		<i>Loft Historic District North</i> Centered on Paca, Redwood, and Eutaw Sts.	B-4093	Twelve large 5- to 7-story manufacturing buildings stylistically representative of Romanesque, Victorian, and early modern industrial architecture; brick with cast iron, terracotta, and stone details	1870 to 1915	Listed	A, C	No Effect
46		<i>Heiser, Rosenfeld, and Strauss Buildings (Inner Harbor Lofts I)</i> 32-34 S. Paca St., 36-38 S. Paca St., 40-42 S. Paca St.	B-2325, B-2323, B-2324	A complex of three 6-story brick industrial loft buildings historically known as the Heiser Building, Rosenfeld Building, and Strauss Building; the Romanesque Revival style Heiser Building also incorporates stone and iron, the Rosenfeld Building has overscaled Beaux Arts styling, and the Strauss Building also incorporates terra cotta and cast iron	1886, 1905, and 1887	Listed	A, C	No Effect




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47		<i>Emerson Bromo-Seltzer Tower</i> 312-18 W. Lombard St.	B-38	Multi-story Romanesque tower topped with a four-dial gravity clock and projecting brackets that support an octagonal crenellated tower; made of yellow brick with stone trim	1911	Listed	A, B, C	No Effect
48		<i>Rombro Building</i> 22-24 S. Howard St.	B-2371	6-story brick, stone and terracotta Queen Anne double warehouse loft building with cast-iron storefront	1881	Listed	C	No Effect
49		<i>Wilkens-Robins Building</i> 308-14 W. Pratt St.	B-3598	5-story 6-bay office and warehouse brick building with a cast iron façade	1871	Listed	A, C	No Effect




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50		<i>Howard Street Tunnel</i> Beneath Howard St. from Mt. Royal Station to Camden Station	B-79	7,341 feet long tunnel made of brick with iron-ring centerings shaped in an arch with flooring that is a flat reverse arch; 21 feet 3 inches at extreme height, 27 feet wide, and averages between 50 to 65 feet below the pavement; provided cover for an underground rail connection beneath Howard Street in downtown Baltimore between the Mount Royal and Camden Stations of the Baltimore and Ohio Railroad	1890-95	Listed	A,C	No Effect
51		<i>Business and Government Historic District</i> Roughly bounded by E. Saratoga St., E. Lexington St., W. Falls Ave., Water St., E. Lombard St., and Charles St.	B-3935	Commercial buildings range from small 2- and 3-story storefront buildings to modern 25-story office towers; generally the buildings feature classical detailing, symmetry, brick or smooth stone façades, and a heavy cornice at the roof line; among the most distinctive are the small classical banks, early high rise office buildings, revival buildings, and the courthouses; also grand scaled government buildings, generally classical in decoration; major landscaping features are Monument Square, War Memorial Plaza, and the open space in front of the Fish Market	primarily ca. 1900 to 1925; some earlier and later	Listed <i>(United States Custom House is within this district and also individually listed)</i>	A, C	Adverse Effect



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52		Alex Brown Building 135 E. Baltimore St.	B-117	2-story, brick and marble, Beaux Arts-style bank building. The central entrance has a foliated marble surround, pediment, and 4 fluted pilasters with Ionic capitals.	1901	Listed (also within the Business and Government Historic District)	A,C	No Effect
53		Canton House 300 Water St.	B-3705	4.5-story Colonial Revival-style building; the facade is 7 bays wide and the South Street side elevation is three bays wide. The exterior wall material is marble at the first story level and brick laid in Flemish bond from the second story up.	1923	Listed (also within the Business and Government Historic District)	A, C	No Effect
54		Merchants' National Bank, site (Baltimore Federal Inner Harbor; USF&G) 301 Water St., 37-39 South St., 304-10 E. Lombard St.	B-3687	3-story granite building with Ionic order pilasters and columns; the façade is 7 bays wide, while the side elevations have three bays (façade and significant elevations only remain today)	1894 and 1904 (rebuilt after the Great Baltimore Fire)	Eligible (also within the Business and Government Historic District)	A, C	No Effect




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55		<i>United States Custom House</i> 40 S. Gay St.	B-36	U-shaped granite Beaux-Arts custom house building with a low base course and a rusticated basement carrying a 3-story engaged colonnade; an entablature and balustrade conceal an attic floor; an elaborate pavilion located at the rear elevation	1903-07	Listed (also within the Business and Government Historic District)	A, C	No Effect
56		<i>United States Appraisers' Stores (Appraisers' Stores Building)</i> 103 S. Gay St.	B-4496	8-story Art Deco rectangular massed reinforced concrete United States Appraisers' Stores building clad in brick; building is distinguished by stylized limestone eagles at the corners of the parapets and granite framed main entrance surmounted by a cast aluminum eagle	1932-34	Eligible (also within the Business and Government Historic District)	A, C	No Effect
57		<i>Candler Building (The Coca-Cola Building)</i> 700 E. Pratt St., 111 Market Pl.	B-1002	12-story brick clad concrete industrial building with an 8-story rear portion	1912	Eligible	C	No Effect




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58		<i>Jonestown Historic District</i> 20 lots on ten acres located to the east of Jones Falls; the northern and southern most boundaries are E. Lexington and Pratt Sts., with S. Central Ave. to the east	B-2784	Residential, commercial, institutional, and industrial neighborhood that housed and employed the immigrant working class	late 18 th to 20 th centuries	Eligible	A, C	No Effect
59		<i>Star Spangled Banner Flag House</i> 844 E. Pratt St.	B-15	2.5-story salmon brick Federal row house with two brick chimneys and gabled dormers on the hipped roof	original construction ca. 1793; addition 1820	Listed (also NHL and MHT easement property)	A	No Effect
60		<i>Little Italy Historic District</i> Roughly bounded by South Central Ave. Historic District, Pratt St., President St., and Eastern Ave.	B-5121	A compact residential neighborhood with mostly small 2- and 3-story brick row houses, a good number resided with Formstone; commercial properties interspersed sporadically; also a few institutional properties such as St. Leo's Roman Catholic Church; narrow, short streets with sidewalks and some street trees, and most buildings directly fronting on the sidewalks	ca. 1830 to 1930	Eligible	A, C	No Effect




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61		<i>Eastern Avenue Pumping Station</i> 751 Eastern Ave.	B-1047	A monumental Classical Revival 3 ½ story municipal building faced with orange brick and trimmed with sandstone and granite	1910-11	Eligible	A, C	No Effect
62		<i>President Street Station (Philadelphia, Wilmington & Baltimore Railroad Station)</i> President and Fleet sts.	B-3741	2-story tall Italianate style headhouse with a barrel-vaulted roof	1849-50	Listed	A	No Effect
63		<i>South Central Avenue Historic District</i> Centering Central Ave. between Pratt and Fleet Sts.	B-5058	Approximately eight blocks of a light industrial precinct comprised of brick 2- and 3-story industrial and residential buildings; also several larger buildings such as the Bagby Furniture Company and Strauss Malt House	ca. early 19 th to mid-20 th century	Listed (<i>Bagby Furniture Company Building is within this district and also individually listed</i>)	A	Adverse Effect


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64		<i>Bagby Furniture Company Building</i> 509-21 S. Exeter St.	B-1011	4-story U-shaped brick factory building with corbel details, comprised of three sections	1902-07	Listed (also within the South Central Historic District)	A, B	No Effect
65		<i>Harford Run Headwall and Drain</i> Under S. Central Ave. from just north of Orleans St. to the Inner Harbor	B-5054	Brick and/or ashlar stone arch drain built on an existing canal; the canal had straightened Harford Run, a natural water feature; one of the earliest public works projects undertaken by the City of Baltimore	canal est. ca. early 1820s; canal enclosure between ca. 1870 and 1950s	Eligible	A, C	No Effect
66		<i>Fell's Point Historic District (includes boundary increase)</i> Roughly bounded on the north by Aliceanna St., on the east by Wolfe St., on the south by the Harbor, and on the west by Dallas St.	B-3714	A harborside residential and light industrial community of approximately 75 acres consisting of mainly small 2.5-story houses to house seamen, ships carpenters, sailmakers, and other artisans; also more elaborate 3.5-story houses for owners of the shipyards, prosperous merchants, and sea captains; earlier houses built of wide beaded-edge clapboards, but the majority are of Flemish-bond brick; a large open market square on the water at the foot of Broadway	ca. 1760s to late 19 th century	Listed	A, C	Adverse Effect

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67		<i>Douglass Place</i> 516-24 S. Dallas St.	B-3694	A group of five 2-story brick Italianate alley row houses built by Frederick Douglass	ca. 1892	Listed	A	No Effect
68		<i>Public School No. 25</i> <i>(Captain Henry Fleete School)</i> 511 S. Bond St.	B-3928	T-shaped brick Late Victorian school; two stories high with a ground level basement and a central 3-story Romanesque tower capped by a pyramidal roof	1892	Listed	A, C	Adverse Effect

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69		<i>Canton Historic District</i> District lying east of Fells Point Historic District, south of Eastern Ave., and west of Conkling St., extending south to the waterfront	B-3704	Urban industrial neighborhood, established by the Canton Company, of approximately 91 square blocks of row houses and larger industrial buildings at the southern end (with access to the harbor and the former Philadelphia Wilmington & Baltimore Railroad along Boston Street); includes transportation arteries, narrow alley streets, and numerous courts like Holtzman and Wise; most blocks include some type of business or service, although Eastern Avenue and the 5000 block of S. Conkling Street show the highest concentration; also churches and schools	ca. 1830s to early 20 th century	Listed	A, C	No Adverse Effect
70		<i>Atlantic Southwestern Broom Company</i> 3500 Boston St., 1301 S. Baylis St.	B-992	Brick factory complex with an original 4-story central core	1911 with early historic additions in 1928 and 1934	Eligible (also located within the Canton Historic District)	A, C	No Effect
71		<i>Gunther Brewing Company (Hamm's Brewing Company)</i> 1200, 1211, and 1301 S. Conkling St., 3601, 3701 O'Donnell St. (south of O'Donnell St., bounded by Haven St. to the east and Conkling St. to the west)	B-998	A brewery complex with fifteen masonry buildings (of these, one is a non-contributor) that include brewhouses, boiler room, stable/ice plant, office buildings, wash house, and stock house	1900 to mid-20 th century	Listed	A	No Effect

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72		<i>The National Brewing Company</i> 3601-901 Dillon St. (south of Dillon St., bounded by Conkling St. to the west, Haven St. to the east, and O'Donnell St. to the south)	B-996	A brewery complex with eleven masonry buildings and one corrugated metal building (of these, two are non-contributors) that include a stock house, grain storage building, oil tank building, bottling plants, and three beer cellars; the stock house dominates the property with a 9-story center tower flanked by 7-story wings	1885 to mid-20 th century	Listed	A	No Adverse Effect
73		<i>Union Railroad</i> East of S. Haven St. between Boston St. and Pulaski Hwy.	B-5163	Railroad tracks (north of O'Donnell St. is mostly abandoned) and infrastructure that includes two girder bridge overpasses	railroad est. ca. 1873; bridge overpasses date to post-1930	Eligible	A	No Adverse Effect
74		<i>Crown Cork & Seal Highlandtown Plant Complex</i> 4401 Eastern Ave. (bounded by Philadelphia Wilmington & Baltimore Railroad tracks, O'Donnell St., Eastern Ave., and Greektown neighborhood; boundary includes Cambridge Iron & Metal Co. B-6165)	B-5172	A complex of approximately 40 buildings that range from 1-story to multi-story and nearly all are interconnected with adjoining buildings; primary building is a 5-story Tudor Revival brick building; some of the other buildings have Italianate or Art Deco details; also four massive cork silos	1905 to 1941	Eligible	A, C	No Effect

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75		<i>B & O Railroad – Philadelphia Branch</i> Runs north from O'Donnell to Lombard Sts., then northeast to wye west of the Bayview railyard	B-5168	Active railroad tracks and infrastructure elevated above grade; single-tracked from O'Donnell Street to Gough Street, becomes double-tracked, then reverts back to a single-track at the Bay View Truss Bridge; there are six bridges consisting of one concrete beam bridge, three metal plate girder bridges, and two iron through-truss bridges	railroad ca. 1883-86; bridges ca. 1880s to 1953	Eligible	A	No Adverse Effect
76		<i>Highlandtown Pumping Station</i> 4118 Eastern Ave. (southeast corner of Eastern Ave. and S. Haven St., faces original Eastern Ave. alignment)	B-5171	1-story brick municipal building with cast concrete ornamentation and an attached open terrace on its west side	1930	Eligible	A, C	No Adverse Effect
77		<i>Greektown Historic District</i> Generally bounded by Gough St. and Eastern Ave. (N), Oldham and S. Quail sts. (E), Hudson St. and Foster Ave. (S), B & O Railroad (W)	B-1368	A historically Greek neighborhood consisting predominantly of residential rowhouses (1910s to 1950s), as well as residential/commercial buildings along Eastern Avenue and two churches	1920-1963	Eligible	A	No Adverse Effect

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78		<i>Johns Hopkins Bayview Hospital Campus</i> 4940, 5100, 5520, 5550 Eastern Ave.	B-5176	A large hospital campus with several historic buildings (including an early 20 th century power plant) amid modern facilities; the three stages of historic construction that remain standing: renovated remains of the original 1860s asylum, 1930s Art Deco buildings, and 1950s construction and renovations; the contributing buildings are: (1) Mason F. Lord Building (1866; renovated 1953-54), (2) Alpha Center (ca. 1866), (3) Old Power Plant (ca. 1910), (4) Power Plant (ca. 1934), (5) A Building (1935), (6) B Building (1935), (7) Laundry (ca. 1936), Behavioral Biology Research Center (1937), and NIDA Building (1953)	1860s to 1950s	Eligible	A, C	No Adverse Effect